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STATE OF WISCONSIN
WISCONSIN HIGHWAY COMMISSION

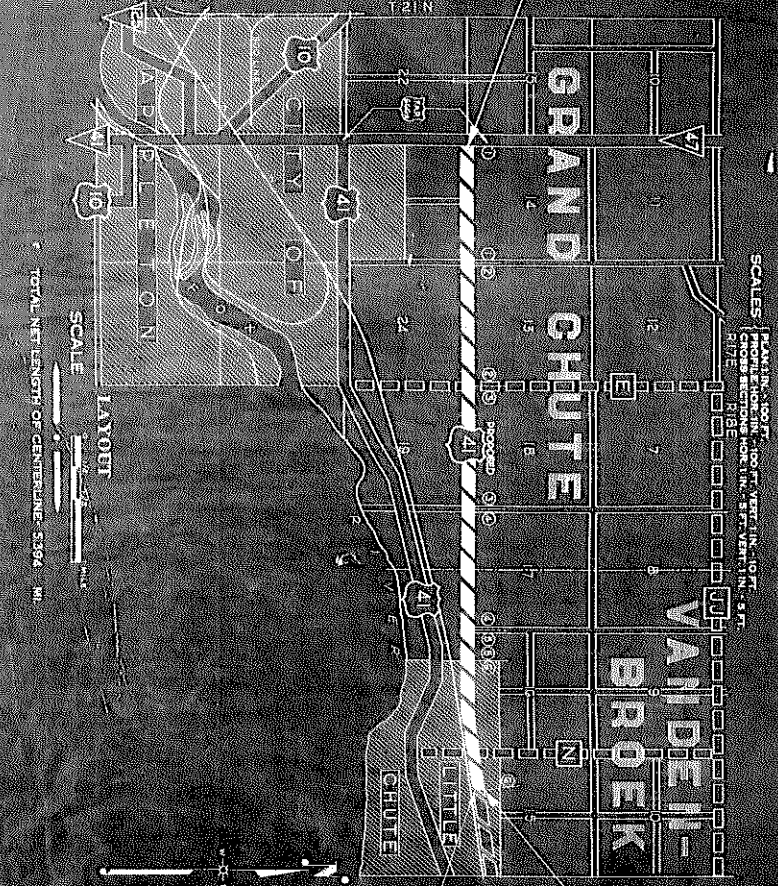
**PLAN AND PROFILE OF PROPOSED
 APPLETON - GREEN BAY ROAD**

APPLETON - LITTLE CHUTE
 OUTAGAMIE COUNTY
 FEDERAL AID PROJECT 582 B

BEG. AT A POINT NEAR THE SW COR. OF SEC. 12, T. 12 N., R. 17 E., S. 17 N. AND EXTENDING EASTERLY TO A POINT NEAR THE SW COR. OF THE SE 1/4 OF SEC. 15, T. 12 N., R. 17 E., S. 17 N. SURVEY STA. 824+32.

CONVENTIONAL SIGNS

STATE LINE	CONVENTIONAL SIGN
CALVERTS/BOURBON	PLATE 101
CLAYTON	PLATE 102
GRAND CHUTE	PLATE 103
VANDEL-BROEK	PLATE 104
LITTLE CHUTE	PLATE 105
APPLETON	PLATE 106
GREEN BAY ROAD	PLATE 107
APPLETON - GREEN BAY ROAD	PLATE 108
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APPLETON - GREEN BAY ROAD	PLATE 110
APPLETON - GREEN BAY ROAD	PLATE 111
APPLETON - GREEN BAY ROAD	PLATE 112
APPLETON - GREEN BAY ROAD	PLATE 113
APPLETON - GREEN BAY ROAD	PLATE 114
APPLETON - GREEN BAY ROAD	PLATE 115
APPLETON - GREEN BAY ROAD	PLATE 116
APPLETON - GREEN BAY ROAD	PLATE 117
APPLETON - GREEN BAY ROAD	PLATE 118
APPLETON - GREEN BAY ROAD	PLATE 119
APPLETON - GREEN BAY ROAD	PLATE 120



CONVENTIONAL SIGNS

STATE LINE	CONVENTIONAL SIGN
CALVERTS/BOURBON	PLATE 101
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APPLETON - GREEN BAY ROAD	PLATE 109
APPLETON - GREEN BAY ROAD	PLATE 110
APPLETON - GREEN BAY ROAD	PLATE 111
APPLETON - GREEN BAY ROAD	PLATE 112
APPLETON - GREEN BAY ROAD	PLATE 113
APPLETON - GREEN BAY ROAD	PLATE 114
APPLETON - GREEN BAY ROAD	PLATE 115
APPLETON - GREEN BAY ROAD	PLATE 116
APPLETON - GREEN BAY ROAD	PLATE 117
APPLETON - GREEN BAY ROAD	PLATE 118
APPLETON - GREEN BAY ROAD	PLATE 119
APPLETON - GREEN BAY ROAD	PLATE 120

SCALE
 TOTAL LENGTH OF CENTERLINE 5.984 MI.

STA. 582+32
 END OF PAVING
 STA. 582+00 OF
 F.A.P. NO. 582-C

STA. 582+49.3
 BEG. OF PAVING
 STA. 582+00 OF
 F.A.P. NO. 582-A

SCALE
 TOTAL LENGTH OF CENTERLINE 5.984 MI.

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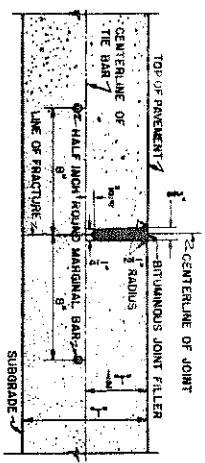
SCALE
 TOTAL LENGTH OF CENTERLINE 5.984 MI.

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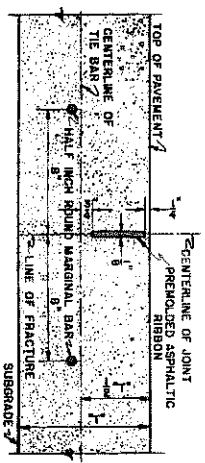
D.S.H. #44

PAVING DETAILS
 RECOMMENDED PAVING
 APPROVED

WISCONSIN HIGHWAY COMMISSION
 STATE ENGINEER

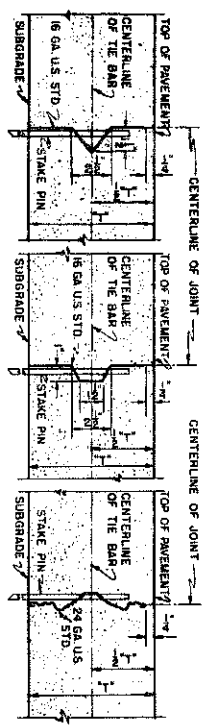
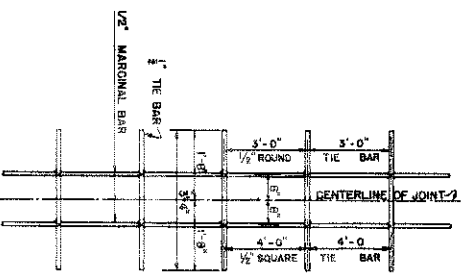


POURED JOINT

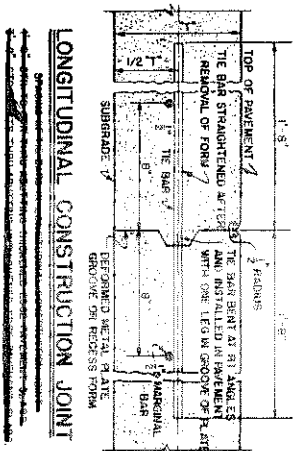


PREFORMED JOINT

ALTERNATE DUMMY JOINTS



ALTERNATE - TYPE "A"



LONGITUDINAL CONSTRUCTION JOINT

CONSTRUCTION NOTES

CONSTRUCTION DETAILS NOT SHOWN HERE SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE SPECIFICATIONS AND APPLICABLE DIMENSION "1" INDICATES PAVEMENT DEPTH ALONG LINE OF LONGITUDINAL JOINT.

MARGINAL BARS SHALL BE DOUBLE CROSS WREED TO THE BARS, LE WIRED IN BOTH DIRECTIONS ACROSS THE INTERSECTION. USE NO. 16 GA. LOOP WIRE TIE.

JOINT ASSEMBLY COMPLETE WITH THE GAINS AND MARGINAL BARS SHALL BE SECURELY PLACED AND HELD IN THE REQUIRED POSITION THROUGH COMPLETE PLACING OPERATIONS.

JOINTS SHALL BE CLEAN CUT AT ALL INTERSECTIONS WITH OTHER JOINTS.

LONGITUDINAL MARGINAL BARS AND DEVELOPED METAL PLATES SHALL NOT EXTEND THRU ANY TRANSVERSE JOINT.

SHIMS TO SUPPORT THE JOINT ASSEMBLY TO THE TRUE GRADE SHALL BE PROVIDED WHERE SUBGRADE CONDITIONS REQUIRE.

BITUMINOUS JOINT FILLER FOR SEALING JOINTS SHALL CONFORM TO THE REQUIREMENTS SPECIFIED FOR TRANSVERSE JOINTS.

FOLLOWING THE INSTALLATION OF THE JOINT OR TEMPORARY FILLER STRIPS FOR CREATING A WEAKENED PLANE, THE FINISHING MACHINE SHALL SCORE OVER THE SECTION AT LEAST ONE TIME.

WEAKENED PLANE TYPE JOINTS SHALL BE FINISHED BY A MECHANICAL METHOD APPROVED BY THE ENGINEER.

JOINTS SHALL NOT DEVIATE MORE THAN 5 PERCENTS FROM THE VERTICAL NOR SHALL THE FACE OF THE JOINT DEVIATE MORE THAN 1/2" EITHER WAY FROM A STRAIGHT LINE AT ANY POINT.

SPACES OR MARGINAL BARS ARE NOT REQUIRED AT LONGITUDINAL JOINTS ALONG THICKENED EDGES OF PAVEMENT SLAB.

DETAILS OTHER THAN THOSE SHOWN TO EFFECT THE SEPARATION OF SLABS AND PROVIDE FOR THE A LOAD TRANSFER ACROSS THE JOINT, COMPARABLE TO THESE DETAILS, MAY BE SUBMITTED TO THE COMMISSION FOR CONSIDERATION & APPROVAL PRIOR TO THEIR USE ON ANY PROJECT.

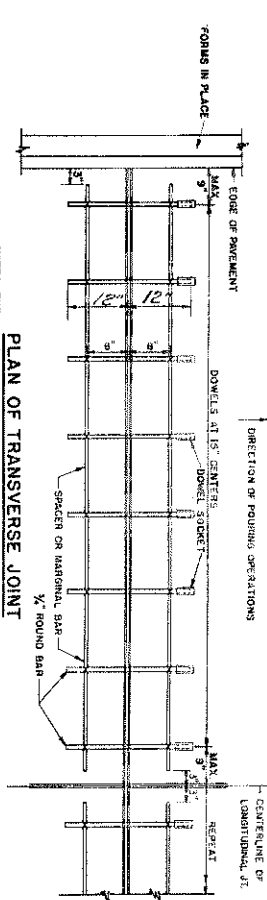
LONGITUDINAL JOINT DETAILS

FOR
CONCRETE PAVEMENT
 STATE HIGHWAY COMMISSION OF WISCONSIN

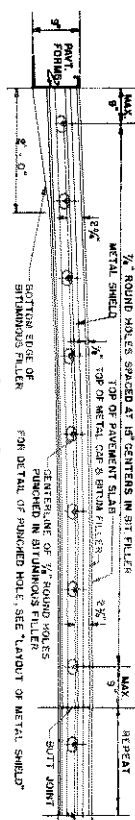
DESIGNED BY: *[Signature]*
 CHECKED BY: *[Signature]*
 DATE: 1/16/37
 STATE ENGINEER

THE BARS AND OTHER ITEMS SHOWN MUST BE SUBMITTED AND APPROVED BY ENGINEER PRIOR TO USE AND SHALL BE SUBJECT TO THE FOLLOWING:
 MAXIMUM SPACING - 3'-0" CENTER TO CENTER FOR 1/2" RD. BAR.
 MAXIMUM SPACING - 4'-0" CENTER TO CENTER FOR 3/4" RD. BAR.
 MAXIMUM SPACING - 5'-0" CENTER TO CENTER FOR 1" RD. BAR.
 MINIMUM AREA STEEL - 1.0 IN. PER LINE FT. OF JOINT OBSOLETE RD. BAR

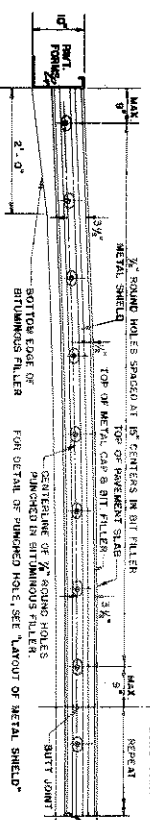
TYPES OF THE BARS



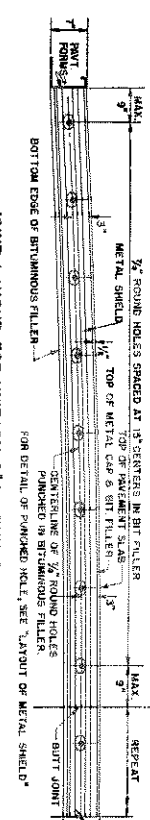
PLAN OF TRANSVERSE JOINT
 NOTE: THE ABOVE DETAILS REFER TO STANDARD DOWEL DESIGN FOR LOAD TRANSFER. METALLATIONS OF OTHER TYPES OF LOAD TRANSFER DEVICES ARE TO CONFORM TO THE DETAILS THEREOF AS APPROVED BY THE DEPARTMENT.



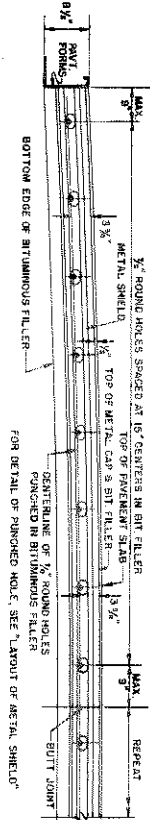
JOINT LAYOUT FOR 9'-6"-9" PAVEMENT



JOINT LAYOUT FOR 10'-8"-10" PAVEMENT

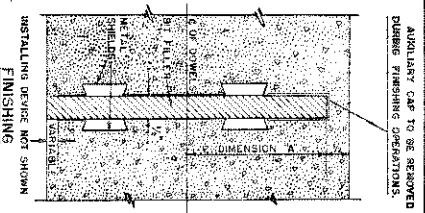


JOINT LAYOUT FOR UNIFORM 7' PAVEMENT

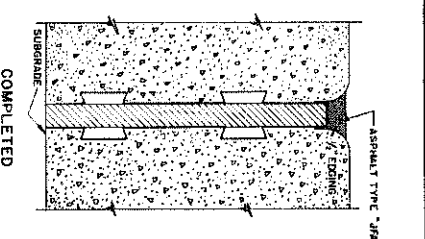


JOINT LAYOUT FOR UNIFORM 8 1/2" PAVEMENT

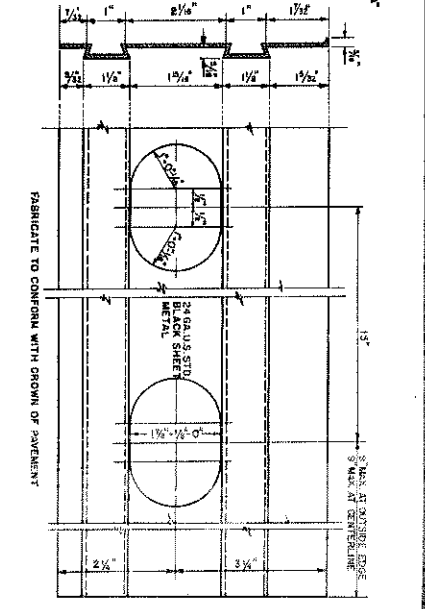
ELEVATIONS OF TRANSVERSE JOINTS



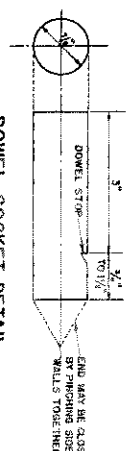
INSTALLATION PRACTICE



COMPLETED



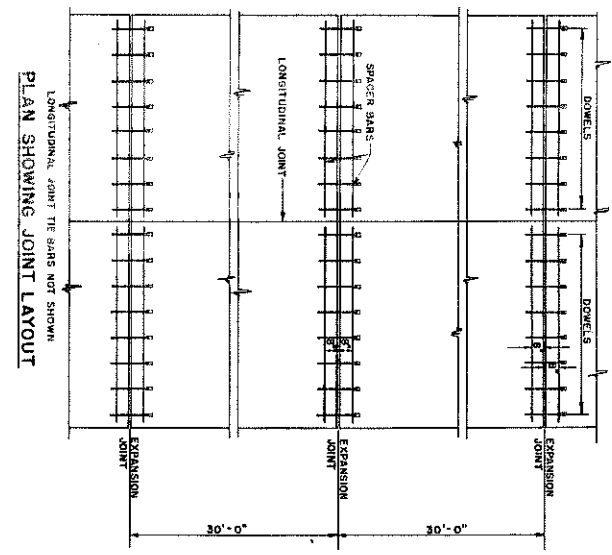
LAYOUT OF METAL SHIELD



DOWEL SOCKET DETAIL

GENERAL NOTES

CONSTRUCTION DETAILS NOT SHOWN HERE SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE SPECIFICATIONS AND OF ALL APPLICABLE SPECIAL PROVISIONS. ALL DOWELS SHALL BE PREMOULDED BITUMINOUS FILLER, 1/2" THICK, SHAPED TO CONFORM TO PERTINENT CROSS SECTION OF PAVEMENT SLAB AND CUT 1/2" BELOW ALL DOWELS SHALL BE TREATED TO BREAK BOND. LENGTH OF SHIELDS SHALL CONFORM TO MINIMUM LENGTH OF SHIELDS TO BE CLOSELY CONFINED IN A MANNER SATISFACTORY TO THE ENGINEER. SHIELDS SHALL BE JOINED TOGETHER IN THE MANNER SHOWN IN THE DRAWINGS. ANY MODIFICATIONS OF DESIGN WILL BE ACCEPTABLE SUBJECT TO THE APPROVAL OF THE ENGINEER. MEANS OF NAILS OR OTHER DEVICES WHICH WOULD TEND TO HOLD SHIELDS TOGETHER PERMANENTLY AFTER CONCRETE HAS SET.



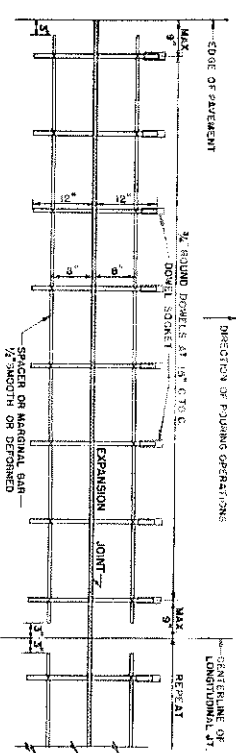
PLAN SHOWING JOINT LAYOUT

STANDARD DESIGN
TRANS. EXPANSION JOINT FOR CONC. PAVEMENT
PREMOULDED BITUMINOUS TYPE

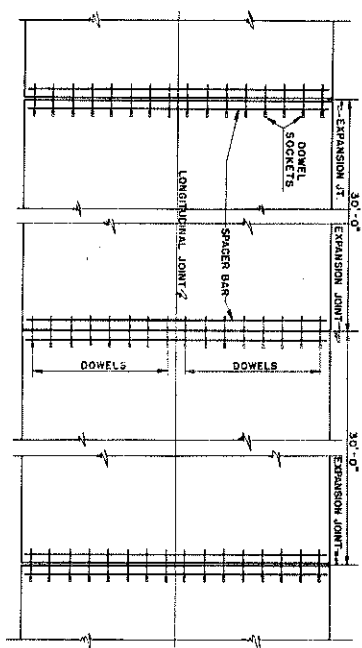
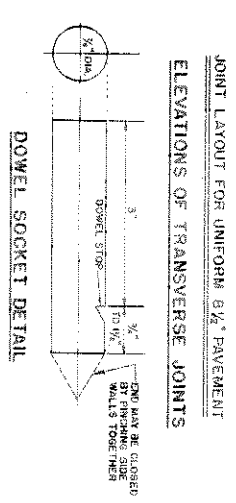
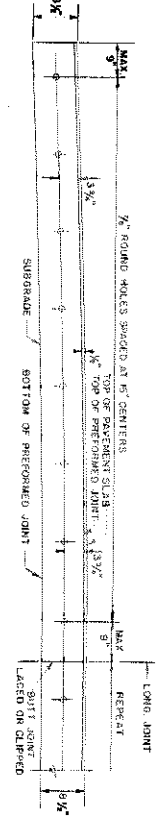
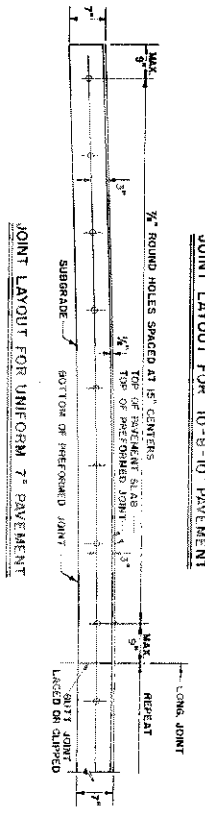
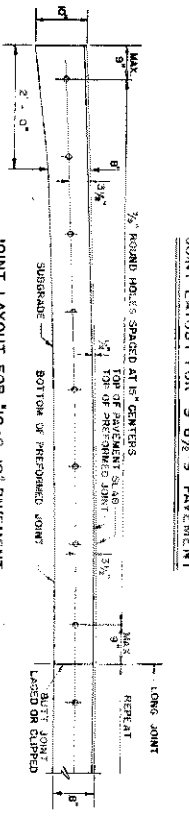
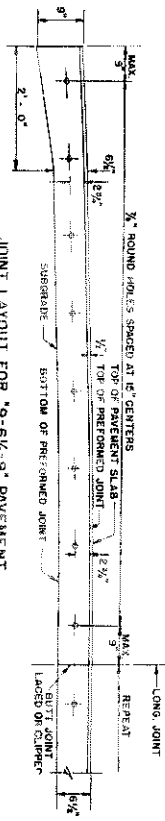
STATE HIGHWAY COMMISSION OF WISCONSIN

RECOMMENDED FOR APPROVAL:
 SPECIAL ENGINEER
 STATE HIGHWAY COMMISSION

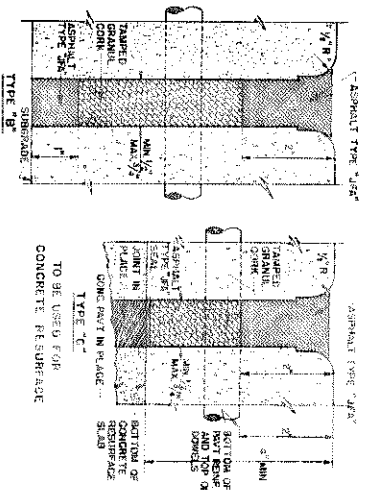
APPROVED:
 STATE HIGHWAY COMMISSION



PLAN OF TRANSVERSE JOINT
 NOTE: THE ABOVE DETAILS REFER TO STANDARD DOWEL DESIGN FOR LOAD TRANSFER. INSTALLATIONS OF OTHER TYPES OF LOAD TRANSFER DEVICES ARE TO CONFORM TO THE DETAILS THEREOF AS APPROVED BY THE DEPT.



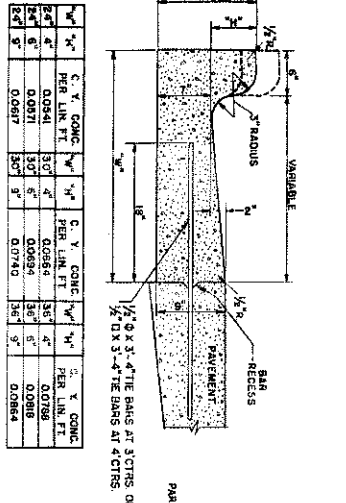
PLAN SHOWING JOINT LAYOUT
 LONGITUDINAL JOINT: THE BARS NOT SHOWN FOR JOINT SPACING ON CONCREIT, REBARING AND DIMENSIONS SEE STD. SPECIFICATIONS



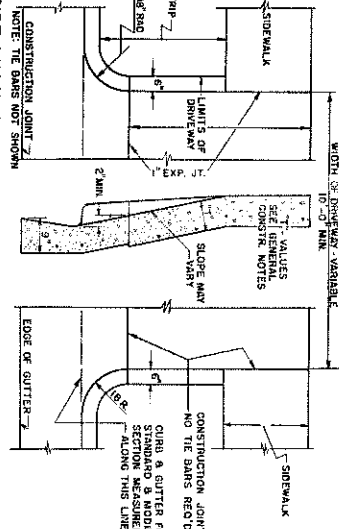
GENERAL NOTES

- CONSTRUCTION DETAILS NOT SHOWN HERE SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE SPECIFICATIONS AND/OR APPLICABLE SPECIAL PROVISIONS.
- MARGINAL OR SPACER BARS SHALL BE 1/2" ROUND, SMOOTH OR DEFORMED, BARS WELDED IN BOTH DIRECTIONS ACROSS THE INTERSECTION. USE NO. 16 BUNGE WIRE THE WIRE.
- NO LONGITUDINAL BARS, REINFORCEMENT OR SALTING STRIP SHALL EXTEND THRU ANY TRANSVERSE JOINT.
- ALL DOWELS SHALL BE TREATED TO BREAK BOND.
- DOWELS FOR CONTRACTION JOINTS IN CONCRETE RESURFACING SHALL BE PLACED 2" BELOW TOP OF NEW SLAB AND DIRECTLY BENEATH PAVEMENT REINFORCEMENT.

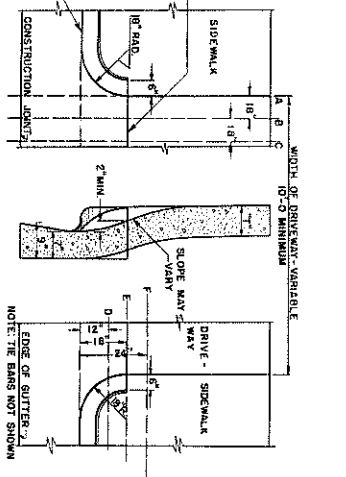
STANDARD DESIGN PLAN	TRANSVERSE JOINTS
RESILIENT TYPE	CAST IN PLACE
RECOMMENDED FOR ADOPTION	STATE HIGHWAY COMMISSION OF WISCONSIN
APPROVED:	
DATE: 3/26/47	



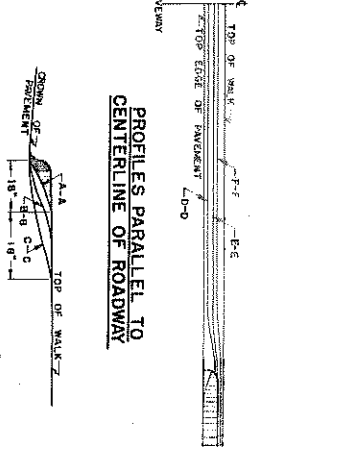
STANDARD SECTION



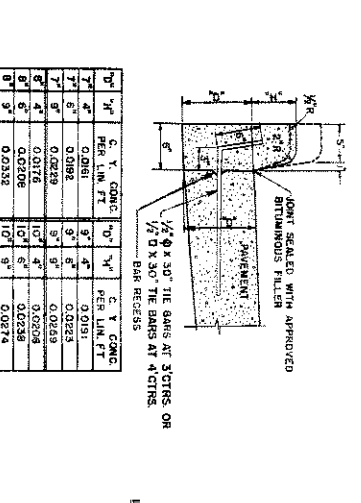
SIDE WALK SEPARATED FROM CURB BY PARK STRIP



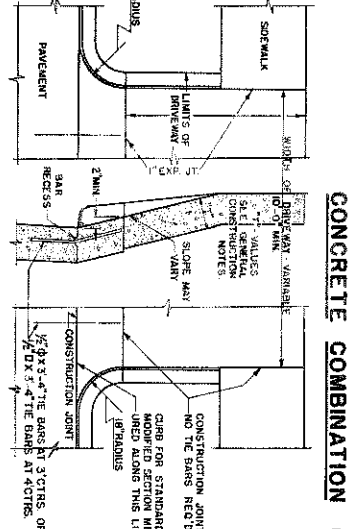
CONCRETE COMBINATION CURB AND GUTTER



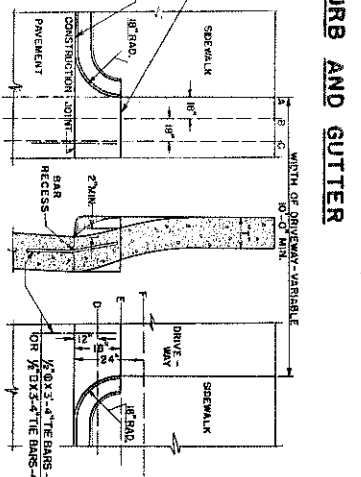
SIDE WALK ADJACENT TO CURB



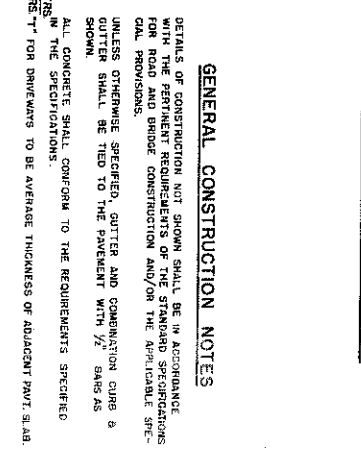
MODIFIED PLAN AND SECTION AT ALLEYS AND PRIVATE ENTRANCES



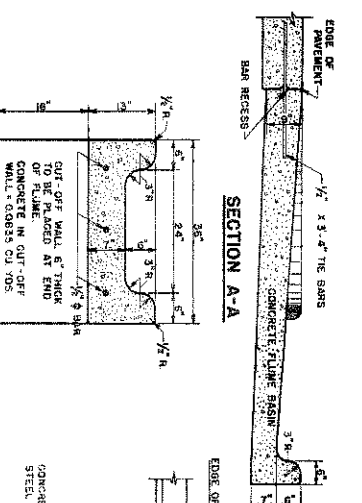
SIDE WALK ADJACENT TO CURB



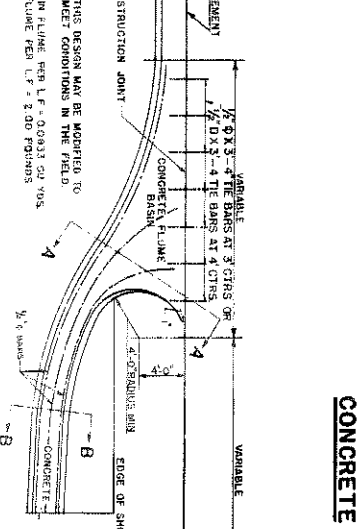
SIDE WALK SEPARATED FROM CURB BY PARK STRIP



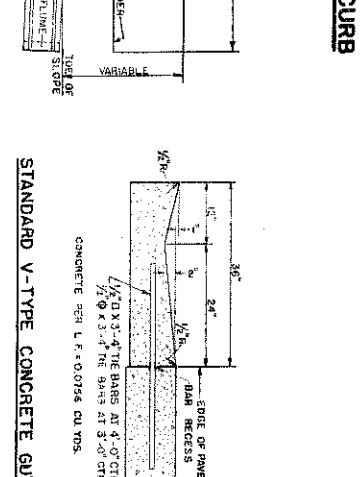
STANDARD CURB



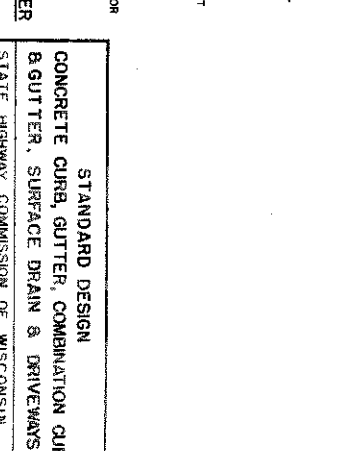
STANDARD CURB



STANDARD CURB



STANDARD CURB



STANDARD CURB

SECTION A-A

SECTION B-B

SECTION C-C

SECTION D-D

SECTION E-E

SECTION F-F

SECTION G-G

SECTION H-H

SECTION I-I

SECTION J-J

SECTION K-K

SECTION L-L

SECTION M-M

SECTION N-N

SECTION O-O

SECTION P-P

SECTION Q-Q

SECTION R-R

SECTION S-S

SECTION T-T

SECTION U-U

SECTION V-V

SECTION W-W

SECTION X-X

SECTION Y-Y

SECTION Z-Z

SECTION AA-AA

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SECTION VV-VV

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SECTION YY-YY

SECTION ZZ-ZZ

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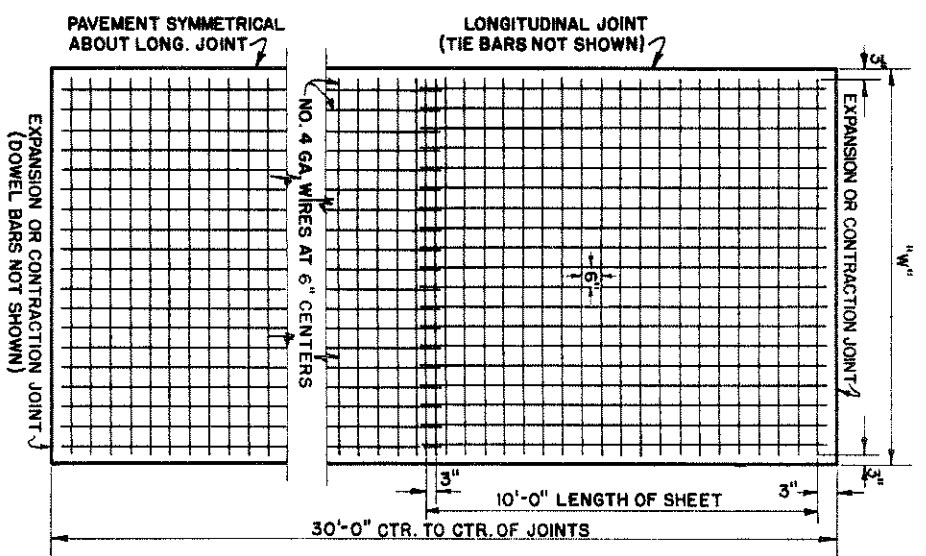
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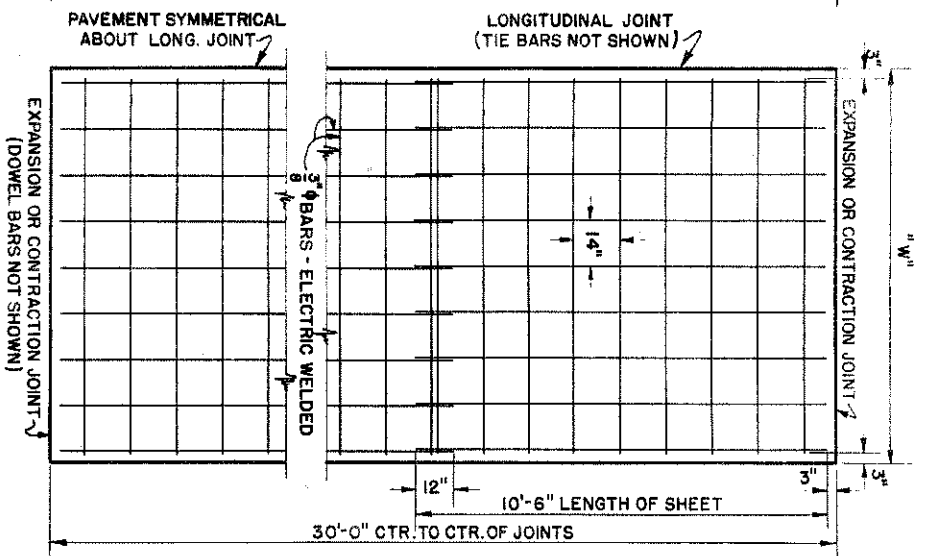
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SECTION AA-AA



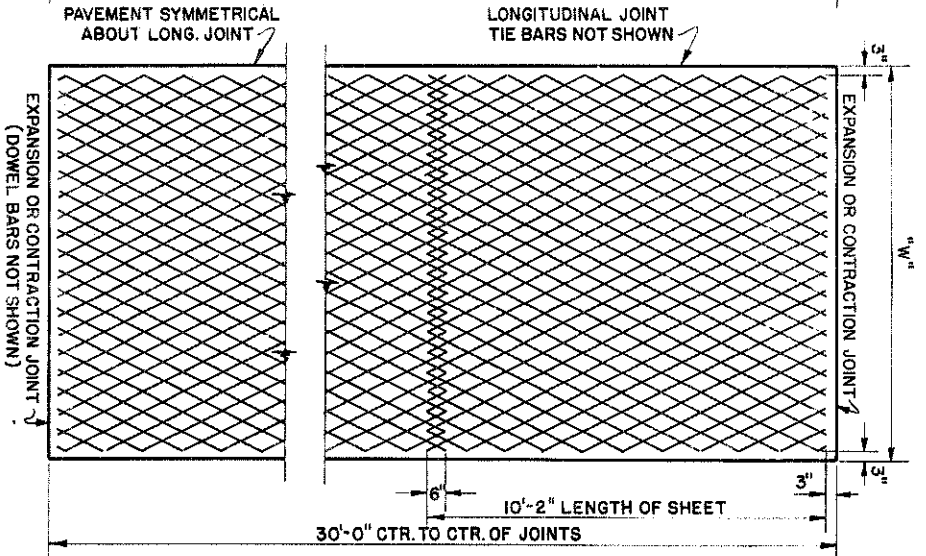
**STEEL FABRIC REINFORCEMENT
ALTERNATE TYPE "A"**

NOTE:
THE DIAMETER OF ALL MEMBERS SHALL BE NOT LESS THAN 0.2253 INCHES (NO. 4 GAUGE).
LONGITUDINAL AND TRANSVERSE MEMBERS OF THE FABRIC SHALL BE SPACED SIX (6) INCHES CENTER TO CENTER AND BE RIGIDLY WELDED AT ALL INTERSECTIONS.



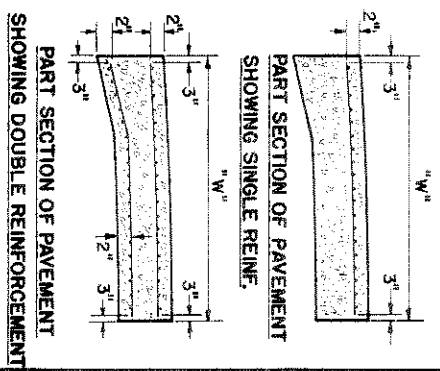
**BAR MAT REINFORCEMENT
ALTERNATE TYPE "B"**

NOTE:
LONGITUDINAL AND TRANSVERSE MEMBERS SHALL BE NOT LESS THAN THREE-EIGHTHS (3/8) INCH ROUND, SMOOTH STEEL RODS SPACED FOURTEEN (14) INCHES CENTER TO CENTER AND RIGIDLY WELDED AT ALL INTERSECTIONS. ALTERNATE METHODS OF FASTENING AT JOINTS OF BARS MAY BE CONSIDERED SUBJECT TO SPECIFIC APPROVAL BY THE DEPARTMENT.



**EXPANDED METAL MESH REINFORCEMENT
ALTERNATE TYPE "C"**

NOTE:
REINFORCEMENT SHALL CONSIST OF DIAMOND SHAPED STEEL MESH.
THE SIZE OF DIAMOND SHALL BE NOT LESS THAN 5" BY 12" NOR MORE THAN 6 1/2" BY 12". UNLESS OTHERWISE SPECIFIED, THE EXPANDED METAL MESH SHALL WEIGH 56 LBS. PER 100 SQ. FT.



GENERAL NOTES

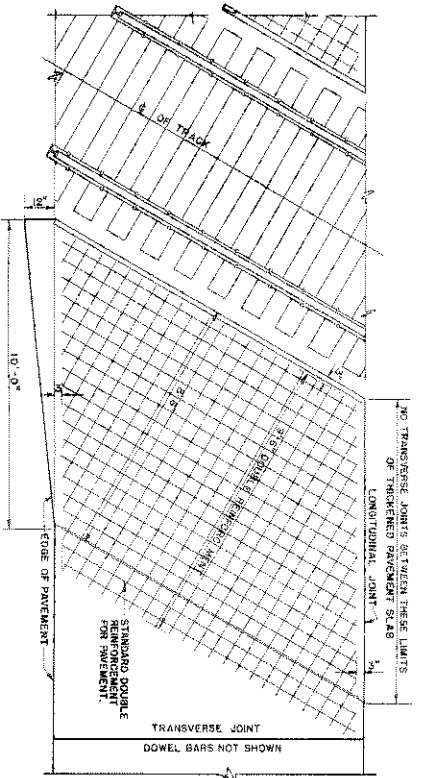
DETAILS NOT COVERED ON THE PLAN SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND/OR APPLICABLE SPECIAL PROVISIONS.
THE USE OF SLEDS TO SUPPORT THE REINFORCEMENT WILL NOT BE PERMITTED. ALL REINF. MUST BE SHIPPED FROM THE FACTORY AND DELIVERED ON THE SITE OF THE WORK IN FLAT SHEETS. SIDE LAPS FOR ADJACENT SHEETS SHALL BE THE SAME AS SHOWN ON THE PLAN FOR END LAPPING.
DIMENSION "W" REPRESENTS WIDTH OF PAVEMENT SLAB BETWEEN LONGITUDINAL JOINTS OR BETWEEN LONGITUDINAL JOINT AND EDGE OF PAVEMENT.

PLANS OF ALTERNATE TYPES OF STEEL REINFORCEMENT FOR CONC. PAVEMENT

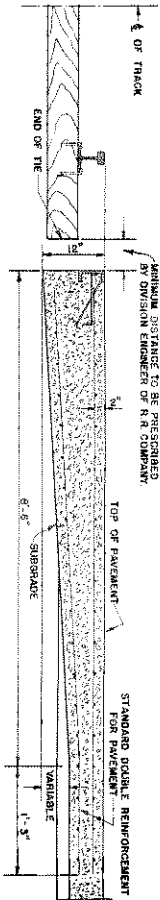
STATE HIGHWAY COMMISSION OF WISCONSIN
STANDARD DESIGN
PAVEMENT REINFORCEMENT
RECOMMENDED FOR APPROVAL:

APPROVED,
David Lorenz
DESIGN ENGINEER
W. J. Hall
CONSTRUCTION ENGINEER

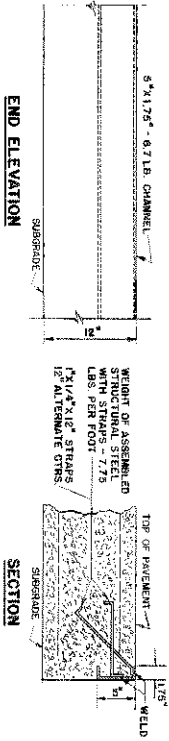
DRAWN RAS 3/26/67
CHECKED ELR DATE
STATE HIGHWAY COMMISSION



PART PLAN OF RAILROAD APPROACH SLABS



SECTION AT RIGHT ANGLES TO TRACK SHOWING REINFORCEMENT



END ELEVATION SECTION

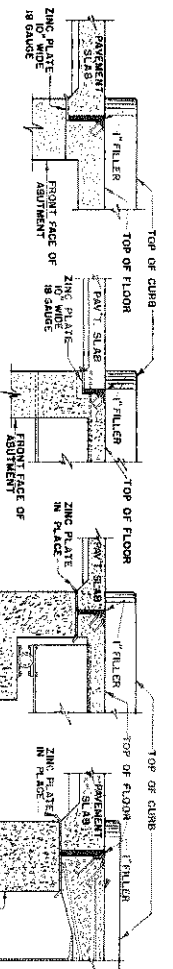
CONSTRUCTION NOTES

STRUCTURAL STEEL & REINFORCEMENT AT R. R. CROSSINGS. PROTECT ENDS OF PAVEMENT AT ALL RAILROAD CROSSINGS AS SHOWN. PAVEMENT TO BE FLAT, GROUN TO BE WORKED OUT IN 25 FEET. ADDITIONAL CONCRETE REQUIRED IN PAVEMENT TO BE CONVERTED INTO SQUARE YARDS OF STANDARD CONCRETE PAVEMENT AND PAID FOR AS SUCH.

STRUCTURAL STEEL IS TO BE MEASURED BY THE POUND AND PAID FOR AT THE CONTRACT UNIT PRICE BID IN THE PROPOSAL FOR STRUCTURAL STEEL FOR PAVEMENT FROM THE CONTRACT UNIT PRICE FOR PAVEMENT FOR FURNISHING COMPLETE IN PLACE.

SUBGRADE TREATMENT FOR R. R. APPROACH SLABS

UNLESS THE SUBGRADE IS COMPOSED OF SAND OR OTHER FREELY SELF-DRAINING MATERIAL, POSITIVE DRAINAGE FOR THE SUBGRADE OF THE SLAB TO A DEPTH OF AT LEAST 18 INCHES AT THE RAILROAD END OF THE PAVEMENT AND RUNNING OUT TO ZERO AT THE FIRST TRANSVERSE JOINT AND BACKFILLING WITH SAND-GRAVEL BACKFILL. TREMOR SHALL BE DRAINED ADEQUATELY THRU THE SHOULDER BY MEANS OF PIPE UNDERDRAINS OR STONE UNDERDRAINS AS SHOWN ON THE PLANS.



SLAB SPANS 6 TO 24 FT. INCL.

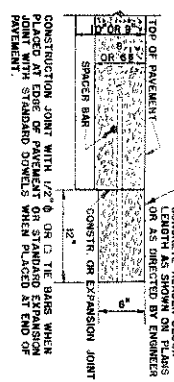
1-BEAM SPANS 8 TO 45 FT. INCL. DECK GIRDER SPANS 20 TO 45 FT. INCL. ARE SIMILAR.

1-BEAM SPANS 50 TO 60 FT. INCL. DECK GIRDER SPANS 50, 55 & 60 FT. INCL. ARE SIMILAR.

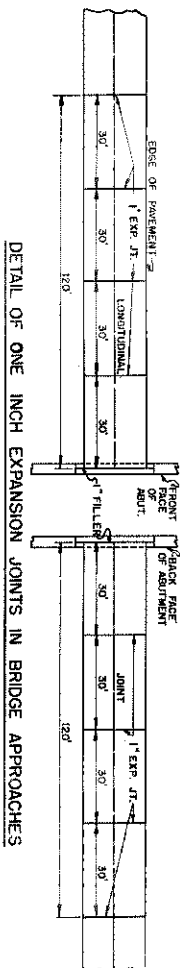
STEEL TRUSS AND PLATE GIRDERS

METHODS OF CONNECTING CONCRETE PAVEMENT WITH BRIDGE STRUCTURES

NOTE: DETAILS TO BE REDUCED OR OBTAINING JOINT OR EQUIVALENT EXPANSION JOINT FILLER ALSO TO BE PLACED BETWEEN CURB ENDS ON OTHER PROTECTING APPROPRIANCES IN THE EVENT PAVEMENT IS PLACED IN CONTACT THEREWITH.



CONCRETE HEADER BLOCK



DETAIL OF ONE INCH EXPANSION JOINTS IN BRIDGE APPROACHES

STANDARD DESIGN R.R. APPROACH SLAB CONG. HEADER BLOCK & PAVT. CONNECTIONS WITH BRIDGES

STATE HIGHWAY COMMISSION OF WISCONSIN

RECOMMENDED FOR APPROVAL

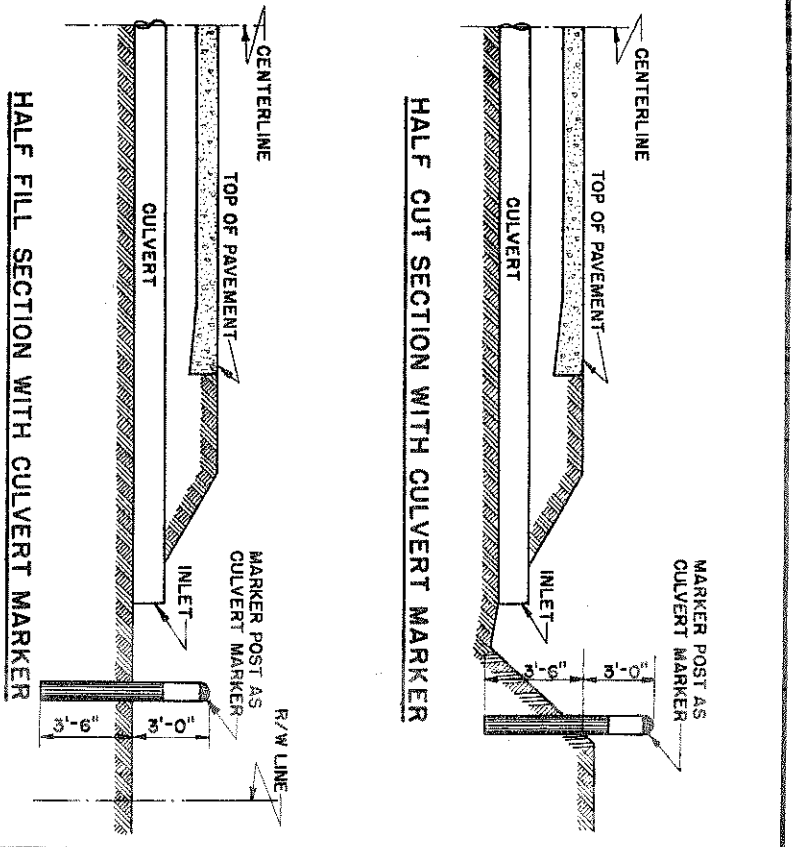
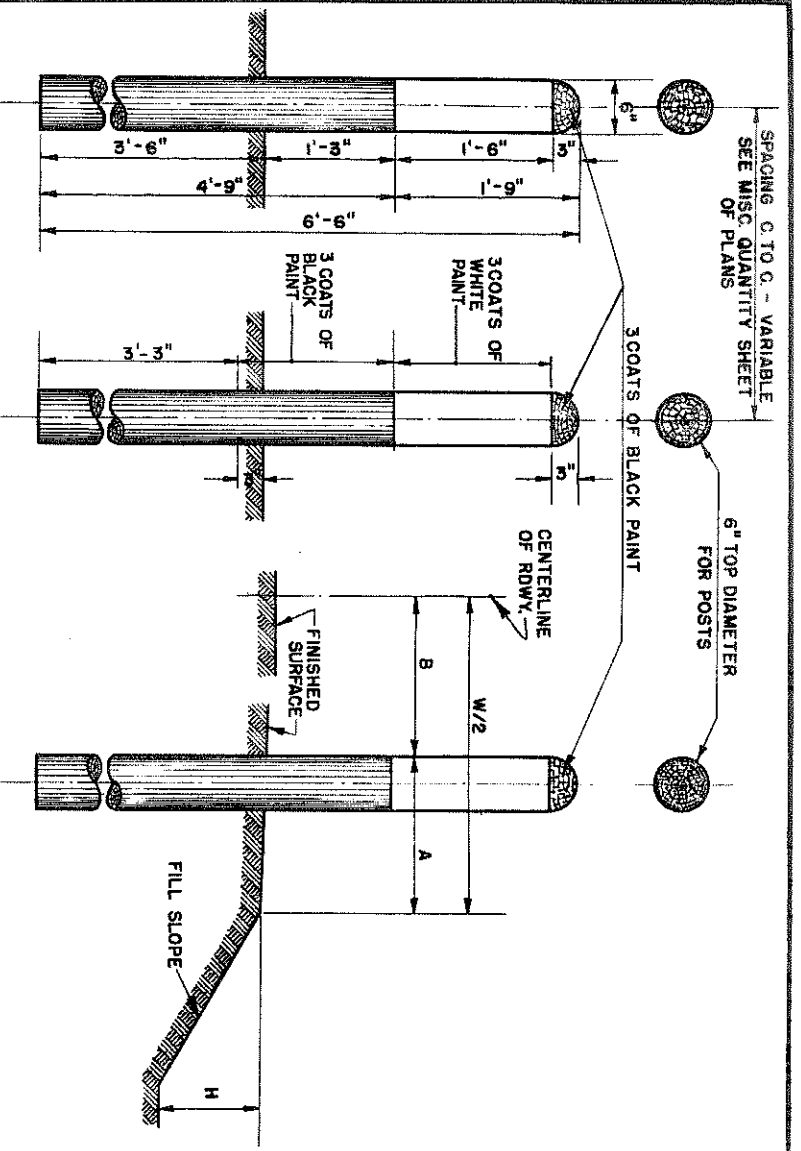
DESIGN ENGINEER

CONSTRUCTION ENGINEER

APPROVED

DATE

DATE WORKING DRAWING



ELEVATION SHOWING SPACING

ELEVATION SHOWING LOCATION ON ROAD SHOULDER

HALF CUT SECTION WITH CULVERT MARKER

HALF FILL SECTION WITH CULVERT MARKER

NOTE:
 MARKER POST AS CULVERT MARKER TO BE PLACED AT INLET END OF CULVERT.
 PLACE POST CLEAR OF CHANNEL.
 IN CUT, PLACE POST ON BACK SLOPE.
 IN FILL, PLACE POST BEYOND END OF CULVERT SO IT MAY BE VISIBLE FROM THE ROADWAY.
 MARKER POSTS FOR RIGHT OF WAY MARKERS SHALL BE PLACED WHERE DIRECTED BY THE ENGINEER.

TABLE OF DIMENSIONS

HEIGHT OF FILL	30' RDWY.			36' RDWY.			40' RDWY.		
	B	A	W/2	B	A	W/2	B	A	W/2
UNDER 5 FT.	15'	2'-6"	17'-6"	18'	2'-6"	20'-6"	20'	2'-6"	22'-6"
5 FT. TO 10'	15'	3'-6"	18'-6"	18'	3'-6"	21'-6"	20'	3'-6"	23'-6"
OVER 10 FT.	15'	4'-6"	19'-6"	18'	4'-6"	22'-6"	20'	4'-6"	24'-6"

GENERAL NOTES

TOP OF POST SHALL BE NEATLY ROUNDED.
 POSTS TO BE CUT AND MANUFACTURED FROM LIVE, GREEN, GROWING NORTHERN WHITE CEDAR, YELLOW PINE, NORWAY PINE, OR WHITE PINE, STACKED & SEASONED IN AN APPROVED MANNER.
 PAINT AND APPLICATION SHALL CONFORM TO PERTINENT SPECS.
 PORTION OF POST TO BE PAINTED SHALL BE SHAVED TO THE WHITE BEFORE PAINTING.

MARKER POSTS

STATE HIGHWAY COMMISSION OF WISC.

RECOMMENDED FOR APPROVAL:

APPROVED:

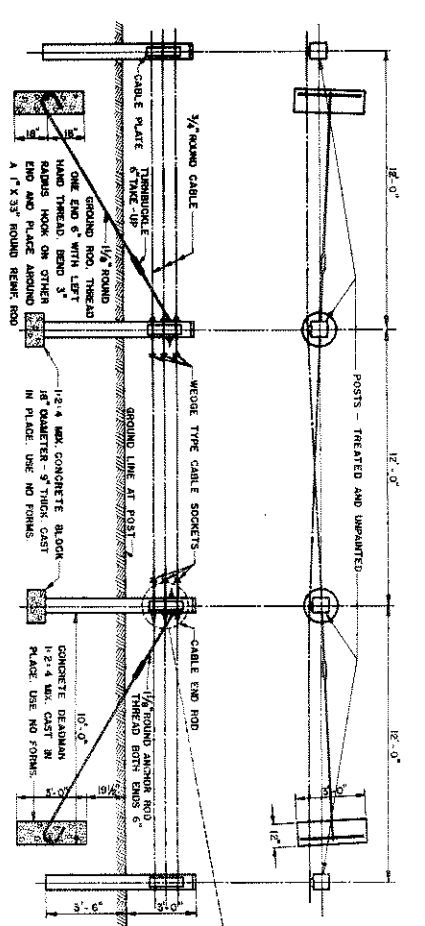
DESIGN ENGINEER

CONSTRUCTION ENGINEER

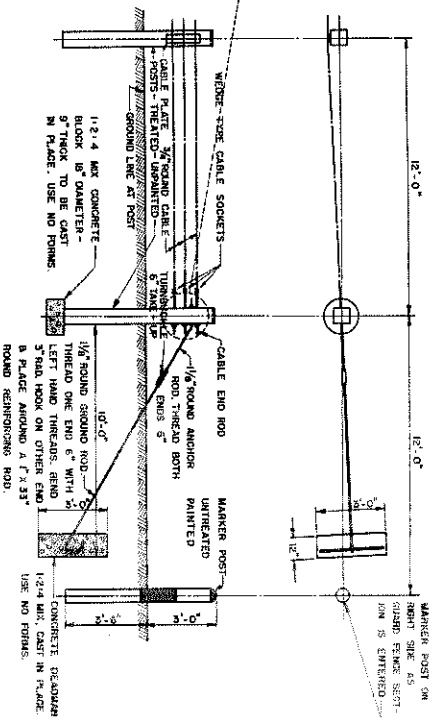
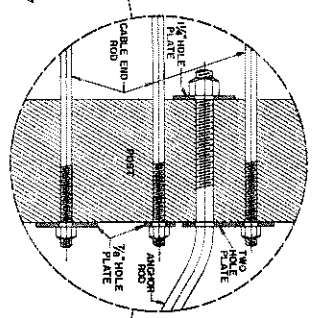
DATE: 3/16/17

DRAWN: fbs

CHECKED: R.F.C.



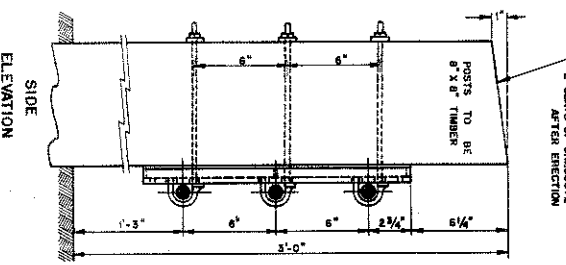
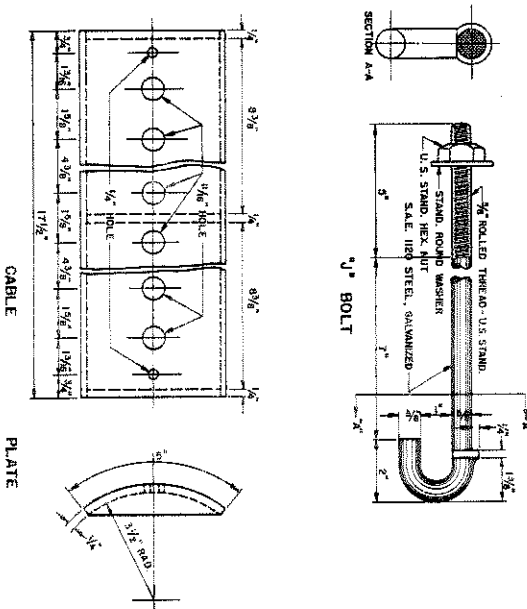
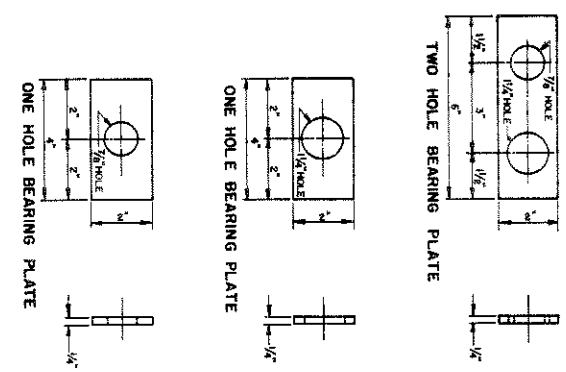
INTERMEDIATE ANCHOR



END ANCHOR

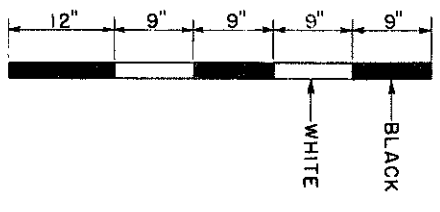
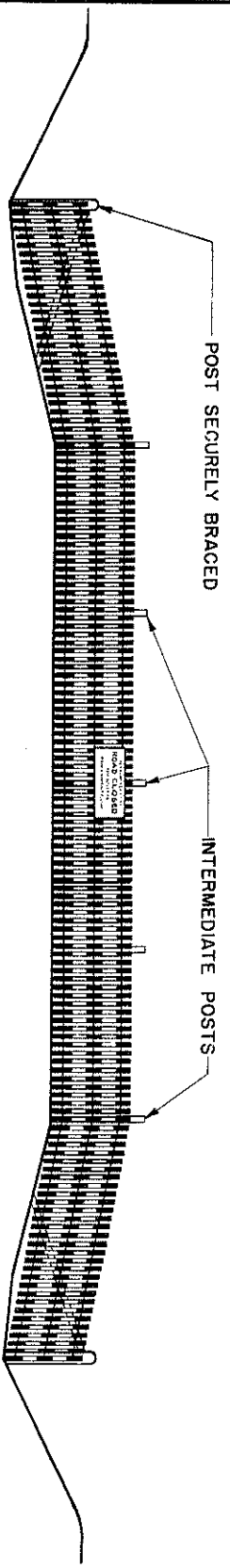
GENERAL CONSTRUCTION NOTES

DETAILS OF CONSTRUCTION NOT COVERED ON THIS SHEET SHALL CONFORM TO THE PERMITS REQUIREMENTS OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION. FOUR DETAILED DRAWINGS OF SOCKETS, TURNBUCKLES AND PLATES MUST BE SUBMITTED TO THE STATE HIGHWAY COMMISSION FOR APPROVAL BEFORE THEIR USE WILL BE PERMITTED. ALL PLATES TO BE WELDED, AND BENTONITE OR GUMMING DURING ERECTION. ALL FITTINGS TO BE GALVANIZED, AND BENTONITE OR GUMMING DURING ERECTION. AFTER RECEPTION, ALL CABLE END BOLTS AND 3\"/>



STANDARD DESIGN FOR
3 WIRE CABLE GUARD FENCE
 STATE HIGHWAY COMMISSION OF WISCONSIN
 RECOMMENDED FOR APPROVAL
 APPROVED: *[Signature]*
 DATE: 3/10 1937
 BY: *[Signature]*

SECTION OF SNOW FENCE TO BE PAINTED ALTERNATELY BLACK AND WHITE AS SHOWN BELOW. LENGTH TO BE SUFFICIENT TO REACH FROM TOE OF THE BACKSLOPE TO TOE OF THE BACKSLOPE. PROVIDE SUFFICIENT NUMBER OF INTERMEDIATE POSTS TO ADEQUATELY SUPPORT THE FENCE. STANDARD "ROAD CLOSED" SIGN (W.H.C. - NO. R-6) TO BE ERECTED ON FENCE AT THE TOP ON THE CENTERLINE OF ROAD.



DETAIL SHOWING METHOD OF STRIPING FOR 4" SNOW FENCE.

STATE HIGHWAY COMMISSION OF WISCONSIN
CONSTRUCTION BARRICADE

RECOMMENDED FOR APPROVAL:
Frank Bauer
 DESIGN ENGINEER
W. M. L. Kelly
 CONSTRUCTION ENGINEER
 4/13/35

APPROVED:
 DRAWN: HM
 CHECKED: WHG
 DATE: 6/13/35
E. G. Feltgen
 STATE HIGHWAY ENGINEER

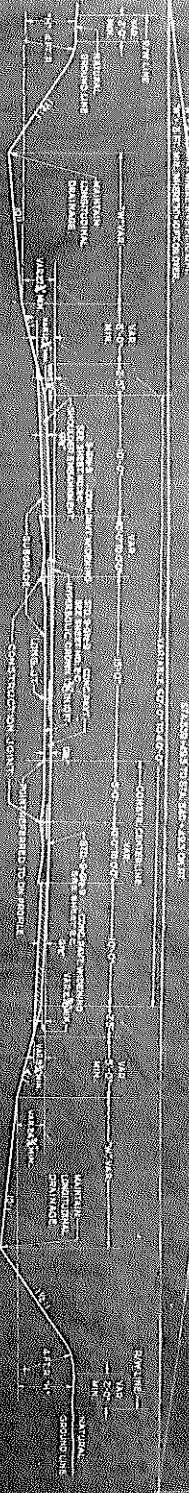
DATE	BY	CHKD	APPD
10/1/55	J.M.B.	J.M.B.	J.M.B.
10/1/55	J.M.B.	J.M.B.	J.M.B.
10/1/55	J.M.B.	J.M.B.	J.M.B.
10/1/55	J.M.B.	J.M.B.	J.M.B.

DIMENSION JOB NO. 1035

TABLE OF V.B.H. VALUES



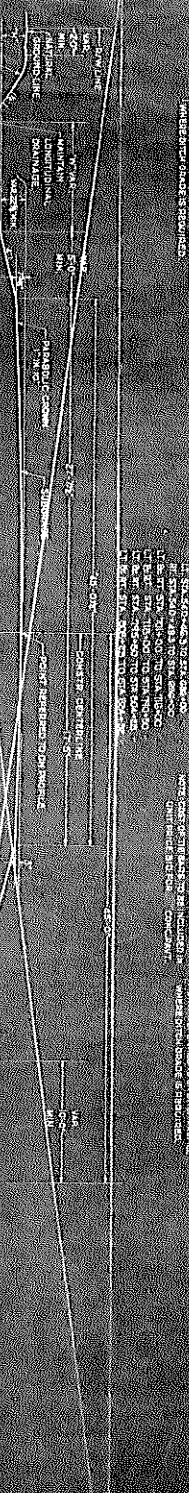
VARIABLE FINISHED PAVEMENT SECTION



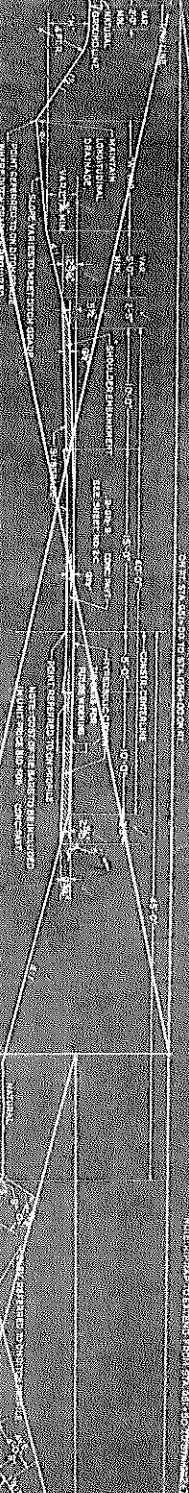
GRADING SECTION



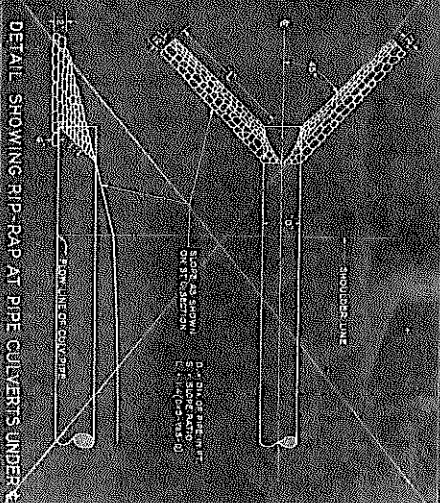
FINISHED PAVEMENT SECTION



GRADING SECTION



FINISHED PAVEMENT SECTION

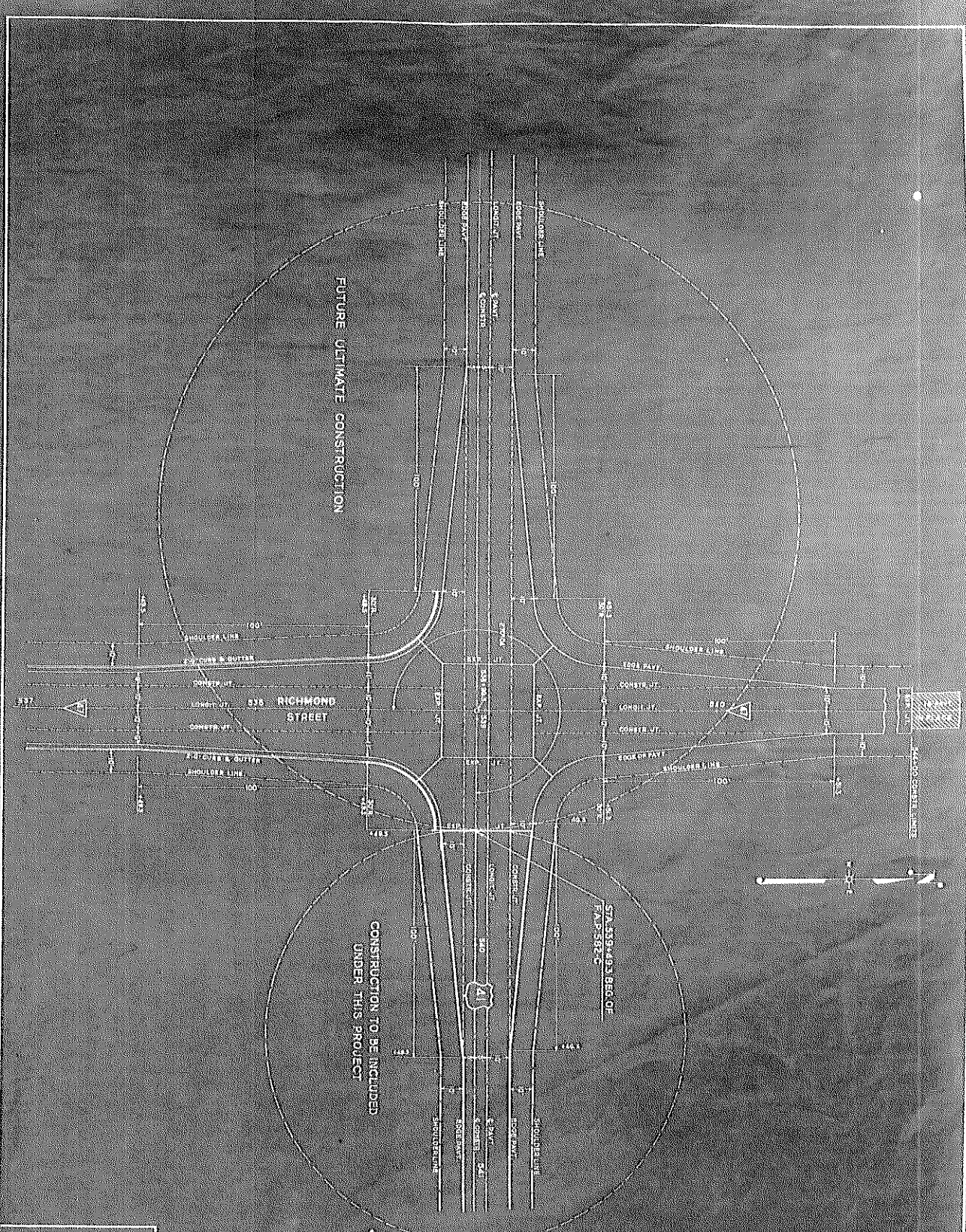


SECTION SHOWING TRENCHING AT CROSSROADS

APPROVED DETAIL DRAWINGS ARE AS FOLLOWS:
 1. TYPICAL CROSS SECTION FOR 40 FOOT ROADBED 20 FT CONCRETE PAVEMENT
 2. SECTION SHOWING TRENCHING AT CROSSROADS
 3. SECTION SHOWING RIP-RAP AT PIPE CULVERT'S UNDER A ROAD
 4. SECTION SHOWING FINISHED PAVEMENT SECTION
 5. SECTION SHOWING GRADING SECTION

TYPICAL CROSS SECTION FOR 40 FOOT ROADBED 20 FT CONCRETE PAVEMENT

SCALE VARIABLE



PROPOSED INTERSECTION
 OF
 U.S.H. 47 & S.T.H. 47
 SCALE
 1" = 40'

21-29

DATE	BY	CHKD.	APP'D.
12/15/58	J.S.	J.S.	J.S.
SHEET NO. 90			
CONSULT. JOB NO. 398			