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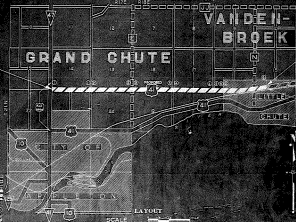
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STATE OF WISCONSIN  
WISCONSIN HIGHWAY COMMISSION

PLAN AND PROFILE OF PROPOSED  
APPLETON - GREEN BAY ROAD  
APPLETON - LITTLE CHUTE  
OUTAGAMIE COUNTY  
582 B FEDERAL AID PROJECT 582 B

860. AT A POINT NEAR THE S.W. COR. OF SEC. 18, T21N, R12E AT SURVEY STA. 539+48.3  
+ STA. 539+48.3  
+ STA. 539+48.3  
OF P.A.P. NO. 582-B  
THE S.E. W. OF THE S.W. W. OF SEC. 18, T21N, R12E AT SURVEY STA. 822-33

SCALE: HORIZONTAL - 1" = 100 FT. VERTICAL - 1" = 10 FT.  
CONVERSIONS: 1" = 10 FT. HORIZONTAL - 1" = 10 FT.  
VERTICAL - 1" = 10 FT.



STA. 539+48.3  
860. AT A POINT NEAR THE S.W. COR. OF SEC. 18, T21N, R12E AT SURVEY STA. 539+48.3  
+ STA. 539+48.3  
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OF P.A.P. NO. 582-B

860. AT A POINT NEAR THE S.W. COR. OF SEC. 18, T21N, R12E AT SURVEY STA. 539+48.3  
+ STA. 539+48.3  
+ STA. 539+48.3  
OF P.A.P. NO. 582-B

## CONVENTIONAL SIGNS

TYPE OF SIGN	DESCRIPTION
STATE LINE	INDICATES BOUNDARY BETWEEN STATES
COUNTY LINE	INDICATES BOUNDARY BETWEEN COUNTIES
SECTION LINE	INDICATES BOUNDARY BETWEEN SECTIONS
OR PART OF WAY LINE	INDICATES BOUNDARY BETWEEN PARTS OF A ROAD
ROAD NAME	INDICATES THE NAME OF A ROAD
ROAD NUMBER	INDICATES THE NUMBER OF A ROAD
ROAD TYPE	INDICATES THE TYPE OF A ROAD
ROAD WIDTH	INDICATES THE WIDTH OF A ROAD
ROAD GRADE	INDICATES THE GRADE OF A ROAD
ROAD SURFACE	INDICATES THE SURFACE OF A ROAD
ROAD CONDITION	INDICATES THE CONDITION OF A ROAD
ROAD MATERIAL	INDICATES THE MATERIAL OF A ROAD
ROAD COLOR	INDICATES THE COLOR OF A ROAD
ROAD MARKING	INDICATES THE MARKING OF A ROAD
ROAD SIGN	INDICATES THE SIGN OF A ROAD
ROAD LIGHT	INDICATES THE LIGHT OF A ROAD
ROAD SOUND	INDICATES THE SOUND OF A ROAD
ROAD TASTE	INDICATES THE TASTE OF A ROAD
ROAD SMELL	INDICATES THE SMELL OF A ROAD
ROAD TOUCH	INDICATES THE TOUCH OF A ROAD
ROAD FEEL	INDICATES THE FEEL OF A ROAD
ROAD LOOK	INDICATES THE LOOK OF A ROAD
ROAD SOUND	INDICATES THE SOUND OF A ROAD
ROAD SMELL	INDICATES THE SMELL OF A ROAD
ROAD TOUCH	INDICATES THE TOUCH OF A ROAD
ROAD FEEL	INDICATES THE FEEL OF A ROAD
ROAD LOOK	INDICATES THE LOOK OF A ROAD

LAYOUT  
SCALE: 1" = 100 FT.

TOTAL NET LENGTH OF CENTERLINE: 2,894 FT.

## PLANNING / ACTUALS

REVISION	DATE	BY	REASON
1	3/28/58	J. H. ...	...
2	...	...	...
3	...	...	...
4	...	...	...
5	...	...	...
6	...	...	...
7	...	...	...
8	...	...	...
9	...	...	...
10	...	...	...

## WISCONSIN DESIGN BY ENGINEERING

PROJ. NO. 582-B

DATE: 3/28/58

BY: J. H. ...

CHECKED BY: ...

APPROVED BY: ...

DATE: 3/28/58

SCALE: 1" = 100 FT.

TOTAL NET LENGTH OF CENTERLINE: 2,894 FT.

...

...

...

...

...

...

...

...

U.S.H. \*4\*

INDEX OF SHEETS

PAVING

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STATE OF WISCONSIN  
WISCONSIN HIGHWAY COMMISSION

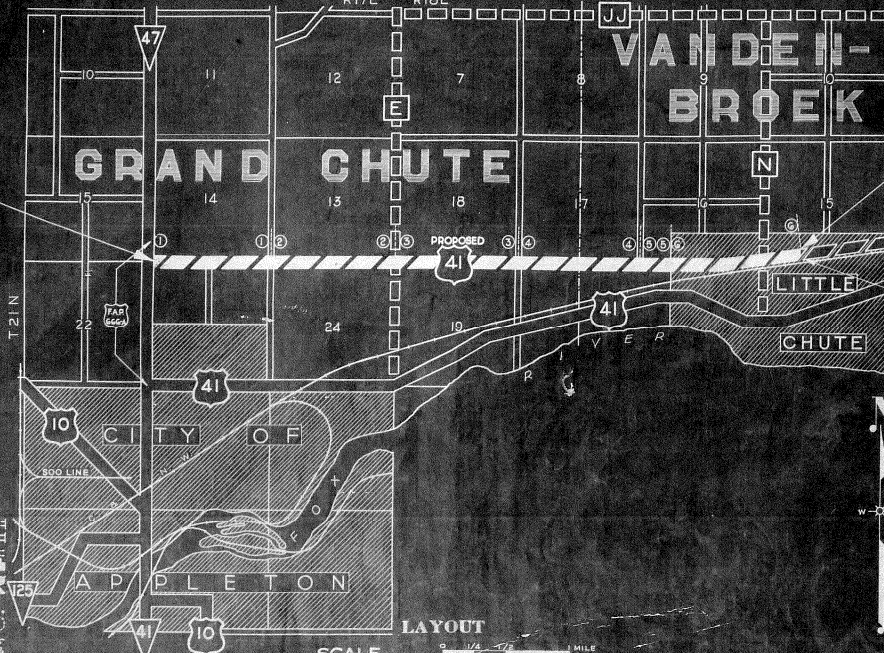
PLAN AND PROFILE OF PROPOSED  
APPLETON - GREEN BAY ROAD  
APPLETON - LITTLE CHUTE  
OUTAGAMIE COUNTY  
582 B FEDERAL AID PROJECT 582 B

BEG. AT A POINT NEAR THE S.W. COR. OF SEC. 14, T21N, R17E, AT SURVEY STA. 539+49.3 AND EXTENDING EASTERLY TO A POINT NEAR THE S.W. COR. OF THE S.E. 1/4 OF THE S.W. 1/4 OF SEC. 15, T21N, R18E, AT SURVEY STA. 824+32.

SCALES | PLAN 1 IN. = 100 FT.  
| PROFILE HOR. 1 IN. = 100 FT. VERT. 1 IN. = 10 FT.  
| CROSS SECTIONS HOR. 1 IN. = 5 FT. VERT. 1 IN. = 5 FT.

STA. 539+49.3  
BEG. OF F.A.P. NO. 582-B  
= STA. 539+49.3  
OF F.A.P. NO. 666-A

STA. 824+32  
END OF F.A.P. NO. 582-B  
= STA. 500+00 OF  
F.A.P. NO. 582-C



CONVENTIONAL SIGNS

STATE LINE	CULVERTS REQUIRED
COUNTY LINE	CULVERTS IN PLACE
TOWNSHIP OR RANGE LINE	DROP INLET
SECTION LINE	POWER POLE
NEW RIGHT OF WAY LINE	TELEPHONE OR TELEGRAPH POLE
PRESENT RIGHT OF WAY LINE	RIGHT OF WAY MARKERS
WIRE FENCE (WOVEN)	REFERENCE STAKE FOR HUBS ONLY
WIRE FENCE (BARBED)	MARSH
LOT LINE	HEDGE
CORPORATE OR CITY LIMITS	TREES
PROPERTY LINE	GROUND ELEVATION
TRAVELED WAY OR P.E.	GRADE ELEVATION
RAILROADS	
BASE OR SURVEY LINE	

SCALE 1/4 MILE

TOTAL NET LENGTH OF CENTERLINE: 5.394 MI.

WISCONSIN HIGHWAY COMMISSION  
MADISON, WIS.

Surveyor: P. H. V. ... Mile Book: 8213, 8889, 8090  
Dir. Compiler: A. L. S. 85-9; M. O. Clarke, M. F. C.  
Dir. Checker: L. B. E. J. E. ... Contd. 10-29-35.

CONTRACT: *S. H. Clavel*  
RECOMMENDED FOR APPROVAL: *Frank Brown* (DESIGN ENGINEER), *W. H. ...* (CONSTRUCTION ENGINEER)  
APPROVED: DATE: 11/1/35  
DATE: 11/1/35

PAVING DETAILS

RECOMMENDED FOR APPROVAL: *W. H. ...*  
DESIGN ENGINEER: *W. H. ...*  
APPROVED: DATE: 1/30/37  
DATE: 1/30/37

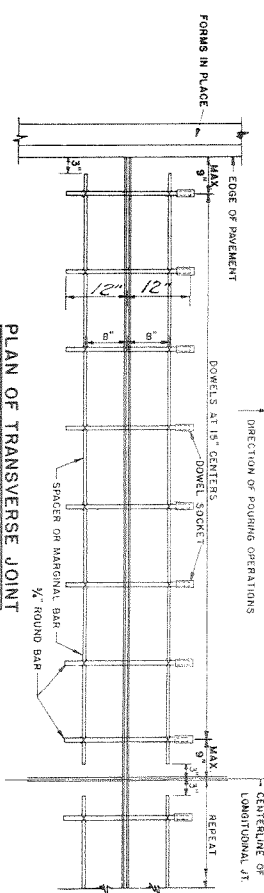
U.S. DEPARTMENT OF AGRICULTURE  
BUREAU OF PUBLIC ROADS

RECOMMENDED FOR APPROVAL: *W. H. ...*  
APPROVED: DATE: 1/30/37  
DATE: 1/30/37

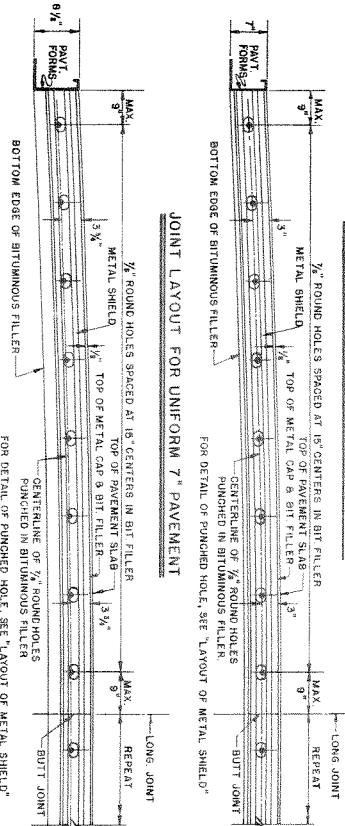
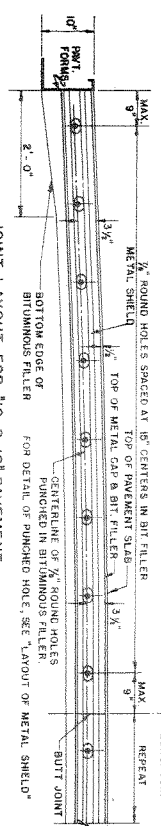
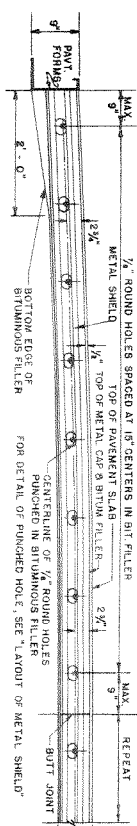
U.S.H. #4

582 B

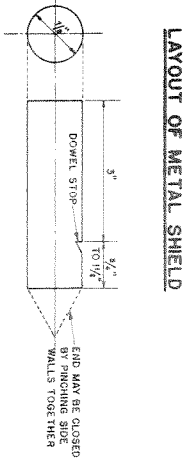
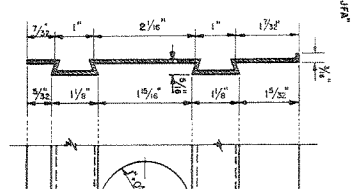
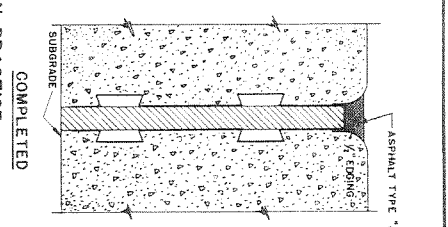
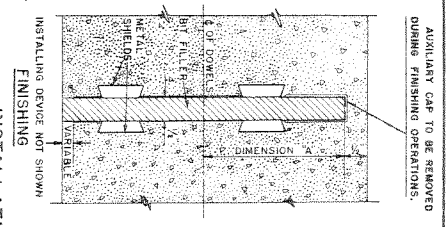




NOTE: THE ABOVE DETAILS REFER TO STANDARD DOWEL DESIGN FOR LOAD TRANSFER. INSTALLATIONS OF OTHER TYPES OF LOAD TRANSFER DEVICES ARE TO CONFORM TO THE DETAILS THEREOF AS APPROVED BY THE DEPARTMENT.

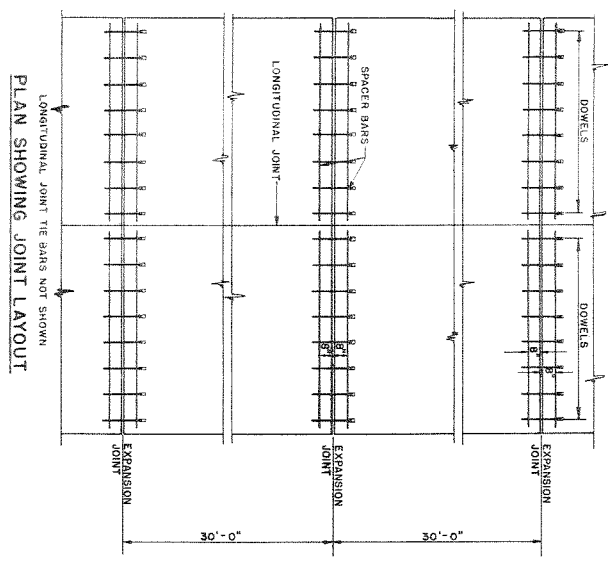


ELEVATIONS OF TRANSVERSE JOINTS



GENERAL NOTES

CONSTRUCTION DETAILS NOT SHOWN HERE SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE SPECIFICATIONS FOR CONCRETE PAVEMENT. ALL SPACER BARS SHALL BE ONE INCH ROUND AND DOUBLE CROSSED TO THE DOWELS. USE WIRE IN BOTH DIRECTIONS ACROSS THE INTERSECTION. NO LONGITUDINAL BARS, REINFORCEMENT OR PARTING STRIP SHALL EXTEND THRU ANY TRANSVERSE JOINT. REFER TO AN APPROVED PREMOLOD BITUMINOUS EXPANSION JOINT FILLER, 1/2" THICK, SHAPED TO CONFORM TO PERTINENT CROSS SECTION OF PAVEMENT SLAB AND CUT 1/2" BELOW ALL DOWELS. LENGTH OF SHIELDS SHALL CONFORM TO MINIMUM LENGTH OF SHIELDS TO BE CLOSED BY CHAINING IN A MANNER SATISFACTORY TO THE ENGINEER. SHALL METHOD OF FASTENING BOTH JOINTS TOGETHER. MODIFICATIONS OF DESIGN WILL BE ACCEPTABLE SUBJECT TO THE APPROVAL OF THE ENGINEER. USE OF MEANS OF NAILS OR OTHER DEVICES WHICH WOULD TEND TO HOLD SHIELDS TOGETHER PERMANENTLY AFTER CONCRETE HAS SET.



STANDARD DESIGN  
TRANS. EXPANSION JOINT FOR CONG. PAVEMENT  
PREMOLOD BITUMINOUS TYPE

STATE HIGHWAY COMMISSION OF WISCONSIN

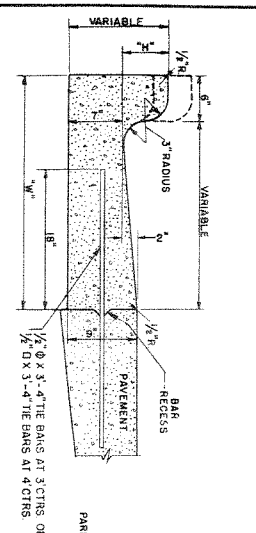
RECOMMENDED FOR APPROVAL:

APPROVED:

DATE: 3/10/37

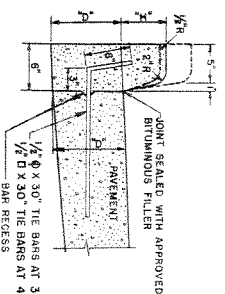
STATE ENGINEER





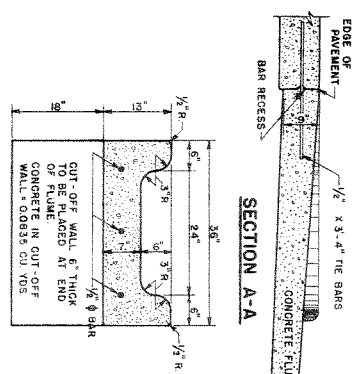
W <sup>1</sup> / <sub>2</sub> "	C Y CONG PER LIN FT	W <sup>1</sup> / <sub>2</sub> "	C Y CONG PER LIN FT	W <sup>1</sup> / <sub>2</sub> "	C Y CONG PER LIN FT	W <sup>1</sup> / <sub>2</sub> "	C Y CONG PER LIN FT
2 1/2"	0.0341	3 0"	0.0364	3 1/2"	0.0388	4 0"	0.0412
2 3/4"	0.0371	3 1/4"	0.0394	3 3/4"	0.0418	4 1/4"	0.0442
3 0"	0.0401	3 3/4"	0.0424	4 0"	0.0448	4 1/2"	0.0472

STANDARD SECTION

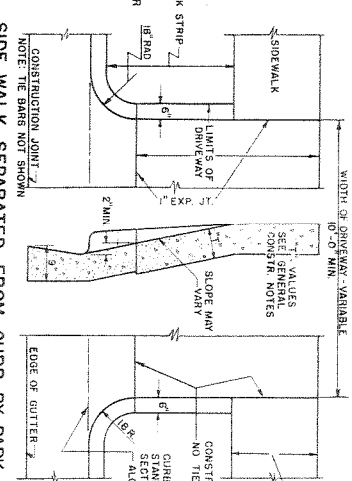


D <sup>1</sup> / <sub>2</sub> "	C Y CONG PER LIN FT	D <sup>1</sup> / <sub>2</sub> "	C Y CONG PER LIN FT	D <sup>1</sup> / <sub>2</sub> "	C Y CONG PER LIN FT	D <sup>1</sup> / <sub>2</sub> "	C Y CONG PER LIN FT
1 1/2"	0.0185	2 0"	0.0210	2 1/2"	0.0235	3 0"	0.0260
1 3/4"	0.0200	2 1/4"	0.0225	2 3/4"	0.0250	3 1/4"	0.0275
2 0"	0.0215	2 1/2"	0.0240	3 0"	0.0265	3 1/2"	0.0290
2 1/4"	0.0230	2 3/4"	0.0255	3 1/4"	0.0280	3 3/4"	0.0305
2 1/2"	0.0245	3 0"	0.0270	3 1/2"	0.0295	4 0"	0.0320

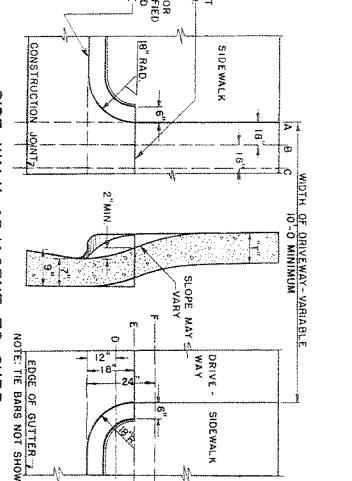
STANDARD SECTION



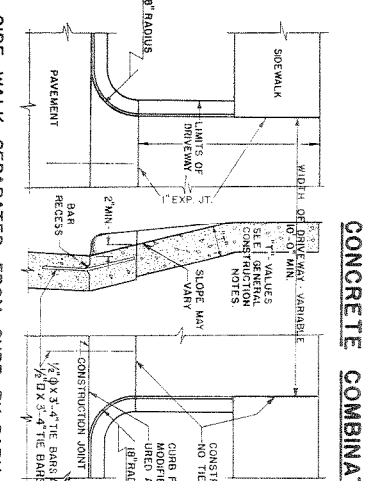
SECTION B-B  
STANDARD CURB AND GUTTER SURFACE DRAIN



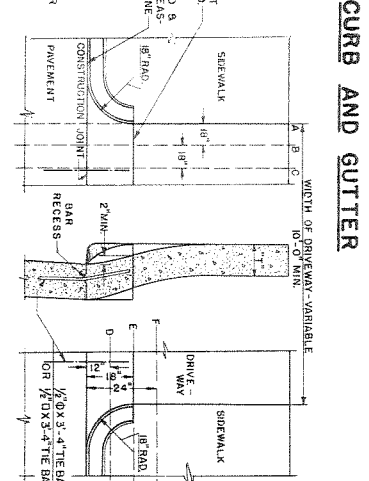
SIDE WALK SEPARATED FROM CURB BY PARK STRIP  
MODIFIED PLAN AND SECTION AT ALLEYS AND PRIVATE ENTRANCES



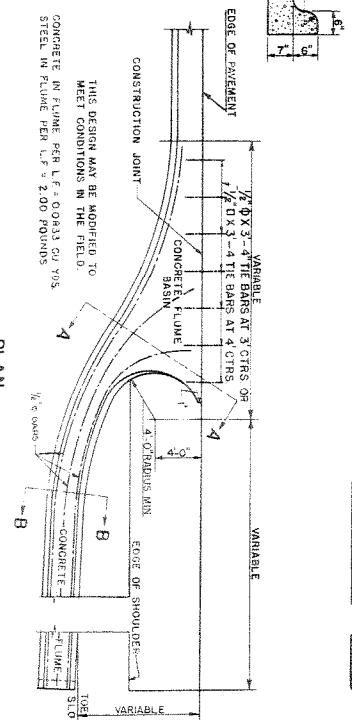
SIDE WALK ADJACENT TO CURB  
MODIFIED PLAN AND SECTION AT ALLEYS AND PRIVATE ENTRANCES



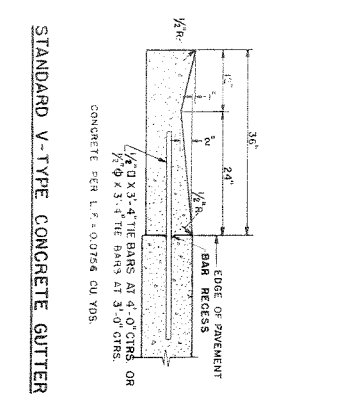
SIDE WALK SEPARATED FROM CURB BY PARK STRIP  
MODIFIED PLAN AND SECTION AT ALLEYS AND PRIVATE ENTRANCES



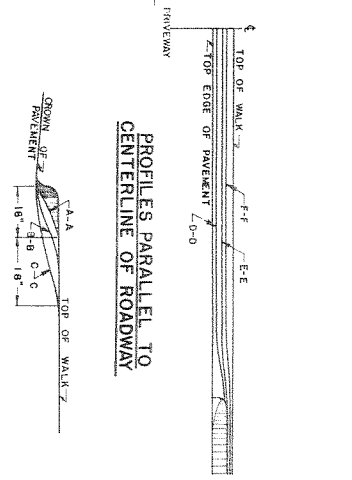
SIDE WALK ADJACENT TO CURB  
MODIFIED PLAN AND SECTION AT ALLEYS AND PRIVATE ENTRANCES



CONCRETE CURB



STANDARD V-TYPE CONCRETE GUTTER



PROFILES PARALLEL TO CENTERLINE OF ROADWAY

GENERAL CONSTRUCTION NOTES

DETAILS OF CONSTRUCTION NOT SHOWN SHALL BE IN ACCORDANCE WITH THE PERTINENT REQUIREMENTS AND/OR THE APPLICABLE SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND/OR THE APPLICABLE SPECIFICATIONS FOR GUTTER AND COMBINATION CURB & GUTTER SHALL BE TIED TO THE PAVEMENT WITH 1/2" BARS AS SHOWN.

ALL CONCRETE SHALL CONFORM TO THE REQUIREMENTS SPECIFIED IN THE SPECIFICATIONS.

FOR DRIVEWAYS TO BE AVERAGE THICKNESS OF ADJACENT PAVT SLAB.

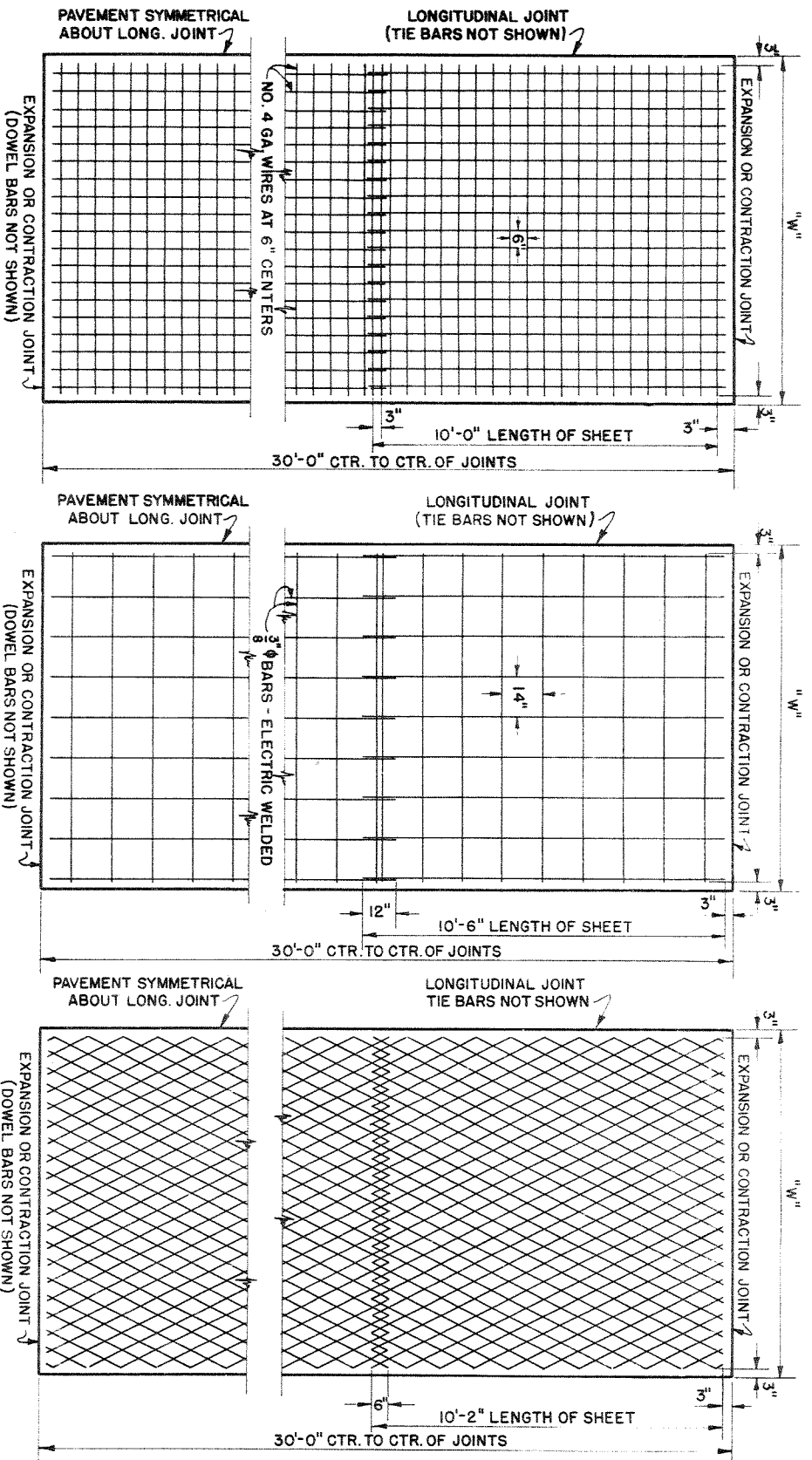
**STANDARD DESIGN**  
CONCRETE CURB, GUTTER, COMBINATION CURB & GUTTER, SURFACE DRAIN & DRIVEWAYS.

STATE HIGHWAY COMMISSION OF WISCONSIN

RECOMMENDED FOR APPROVAL:  
DESIGN ENGINEER: *[Signature]*  
CONSTRUCTION ENGINEER: *[Signature]*

APPROVED:  
STATE HIGHWAY ENGINEER: *[Signature]*

DATE: 3/18/37



**NOTE:**  
THE DIAMETER OF ALL MEMBERS SHALL BE NOT LESS THAN 0.2253 INCHES (NO. 4 GAUGE)

LONGITUDINAL AND TRANSVERSE MEMBERS OF THE FABRIC SHALL BE SPACED SIX (6) INCHES CENTER TO CENTER AND BE RIGIDLY WELDED AT ALL INTERSECTIONS.

**STEEL FABRIC REINFORCEMENT  
ALTERNATE TYPE "A"**

**NOTE:**

LONGITUDINAL AND TRANSVERSE MEMBERS SHALL BE NOT LESS THAN THREE-EIGHTHS INCH ROUND, SMOOTH STEEL RODS SPACED FOURTEEN (14) INCHES CENTER TO CENTER AND RIGIDLY WELDED AT ALL INTERSECTIONS. ALTERNATE METHODS OF FASTENING AT JUNCTIONS OF BARS MAY BE CONSIDERED SUBJECT TO SPECIFIC APPROVAL BY THE DEPARTMENT.

**BAR MAT REINFORCEMENT  
ALTERNATE TYPE "B"**

**NOTE:**

REINFORCEMENT SHALL CONSIST OF DIAMOND SHAPED STEEL MESH.  
THE SIZE OF DIAMOND SHALL BE NOT LESS THAN 5" BY 12" NOR MORE THAN 6 1/2" BY 12" UNLESS OTHERWISE SPECIFIED, THE EXPANDED METAL MESH SHALL WEIGH 56 LBS. PER 100 SQ. FT.

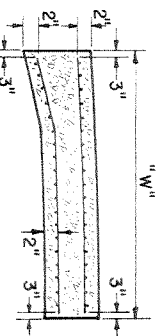
**EXPANDED METAL MESH REINFORCEMENT  
ALTERNATE TYPE "C"**

**GENERAL NOTES**

DETAILS NOT COVERED ON THE PLAN SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND/OR APPLICABLE SPECIAL PROVISIONS.

THE USE OF SLEDS TO SUPPORT THE REINFORCEMENT WILL NOT BE PERMITTED. ALL REINF. MUST BE SHIPPED FROM THE FACTORY AND DELIVERED ON THE SITE OF THE WORK IN FLAT SHEETS. SIDE LAPS FOR ADJACENT SHEETS SHALL BE THE SAME AS SHOWN ON THE PLAN FOR END LAPPING. DIMENSION "w" REPRESENTS WIDTH OF PAVEMENT SLAB BETWEEN LONGITUDINAL JOINTS OR BETWEEN LONGITUDINAL JOINT AND EDGE OF PAVEMENT.

**PART SECTION OF PAVEMENT SHOWING SINGLE REINF.**



**PART SECTION OF PAVEMENT SHOWING DOUBLE REINFORCEMENT**



**PLANS OF ALTERNATE TYPES OF STEEL REINFORCEMENT FOR CONC. PAVEMENT**

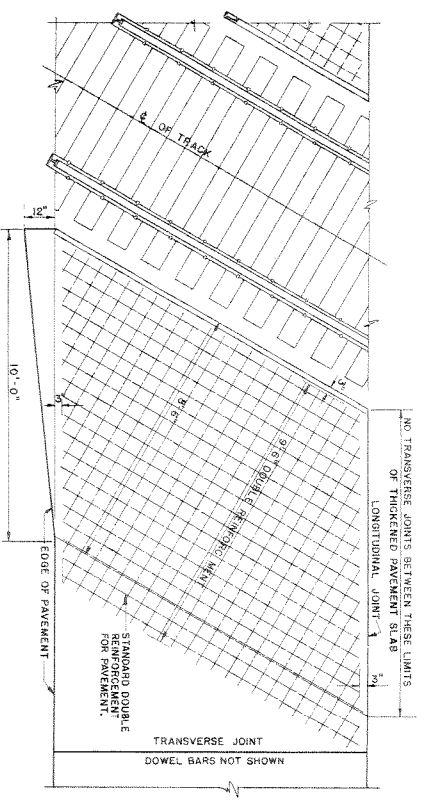
STATE HIGHWAY COMMISSION OF WISCONSIN  
**STANDARD DESIGN  
PAVEMENT REINFORCEMENT**

RECOMMENDED FOR APPROVAL:

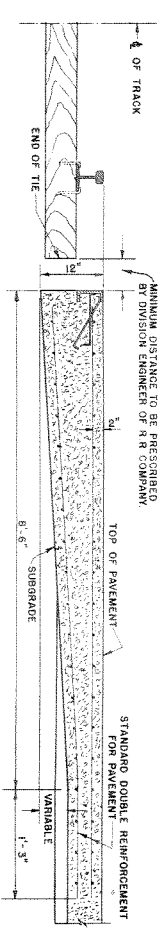
*Frank Green*  
DESIGN ENGINEER  
*W. M. Wood*  
CONSTRUCTION ENGINEER

APPROVED:

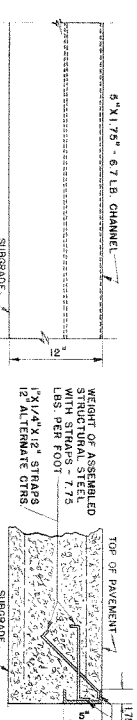
DRAWN: RBS 3/16/37  
CHECKED: ELR DATE: 24/3/37  
STATE HIGHWAY ENGINEER



**PART PLAN OF RAILROAD APPROACH SLABS**



**SECTION AT RIGHT ANGLES TO TRACK SHOWING REINFORCEMENT**



**END ELEVATION SECTION**

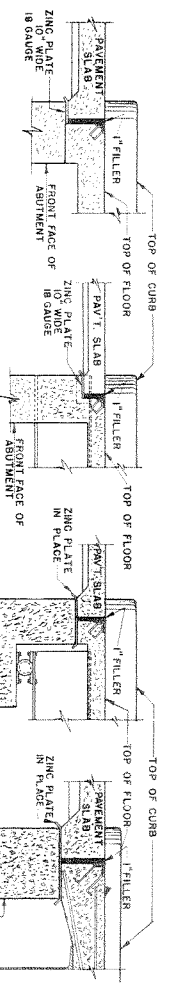
**STRUCTURAL STEEL AT RAILROAD CROSSINGS**

**CONSTRUCTION NOTES**

STRUCTURAL STEEL & REINFORCEMENT AT R.R. CROSSINGS, PROTECT ENDS OF PAVEMENT AT ALL RAILROAD CROSSINGS AS SHOWN. PAVEMENT TO BE FLAT. CROWN TO BE WORKED OUT IN 2 1/2 FEET. ADDITIONAL CONCRETE REQUIRED TO BE CONVERTED INTO SQUARE YARDS OF STANDARD CONCRETE PAVEMENT AND PAID FOR AS SUCH. STRUCTURAL STEEL IS TO BE MEASURED BY THE POUND AND PAID FOR AT THE CONTRACT UNIT PRICE BID IN THE PROPOSAL FOR STRUCTURAL STEEL FOR PAVEMENT. WHICH PRICE SHALL BE FULL COMPENSATION FOR FURNISHING COMPLETE IN PLACE.

**SUBGRADE TREATMENT FOR R.R. APPROACH SLABS**

UNLESS THE SUBGRADE IS COMPOSED OF SAND OR OTHER FREELY SELF-DRAINING MATERIAL, POSITIVE DRAINAGE FOR THE SUBGRADE SHALL BE OBTAINED BY MEANS OF TRENCHING FOR THE FULL WIDTH OF THE SLAB TO A DEPTH OF AT LEAST 18 INCHES AT THE RAILROAD END OF THE PAVEMENT AND RUNNING OUT TO ZERO AT THE FIRST TRANSVERSE JOINT AND BACKFILLING WITH SAND-GRAVEL BACKFILL. TRENCH SHALL BE DRAINED ADEQUATELY THRU THE SHOULDERS BY MEANS OF PIPE UNDERDRAINS OR STONE UNDERDRAINS AS SHOWN ON THE PLANS.



**SLAB SPANS 6 TO 24 FT. INCL.**

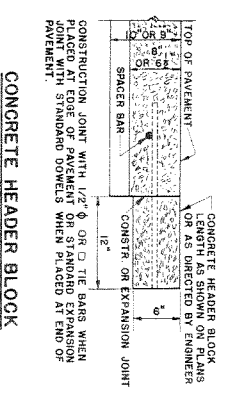
**I-BEAM SPANS 8 TO 45 FT. INCL. DECK GIRDER SPANS 20 TO 45 FT. INCL. ARE SIMILAR**

**I-BEAM SPANS 50 TO 60 FT. INCL. DECK GIRDER SPANS 50, 65 & 80 FT. INCL. ARE SIMILAR**

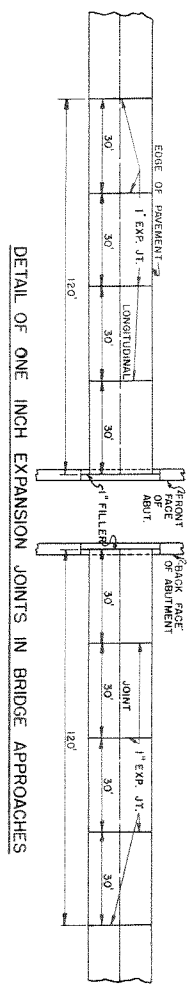
**STEEL TRUSS AND PLATE GIRDERS**

**METHODS OF CONNECTING CONCRETE PAVEMENT WITH BRIDGE STRUCTURES**

NOTE: 1" FILLER TO BE PREMOULDED BITUMINOUS JOINT OR EQUIVALENT. EXPANSION JOINT FILLER ALSO TO BE PLACED BETWEEN CURB ENDS OR OTHER PROJECTING APPURTENANCES IN THE EVENT PAVEMENT IS PLACED IN CONTACT THEREWITH.



**CONCRETE HEADER BLOCK**



**DETAIL OF ONE INCH EXPANSION JOINTS IN BRIDGE APPROACHES**

**STANDARD DESIGN**  
**R.R. APPROACH SLAB CONCRETE HEADER BLOCK & PAV'T. CONNECTIONS WITH BRIDGES**

STATE HIGHWAY COMMISSION OF WISCONSIN

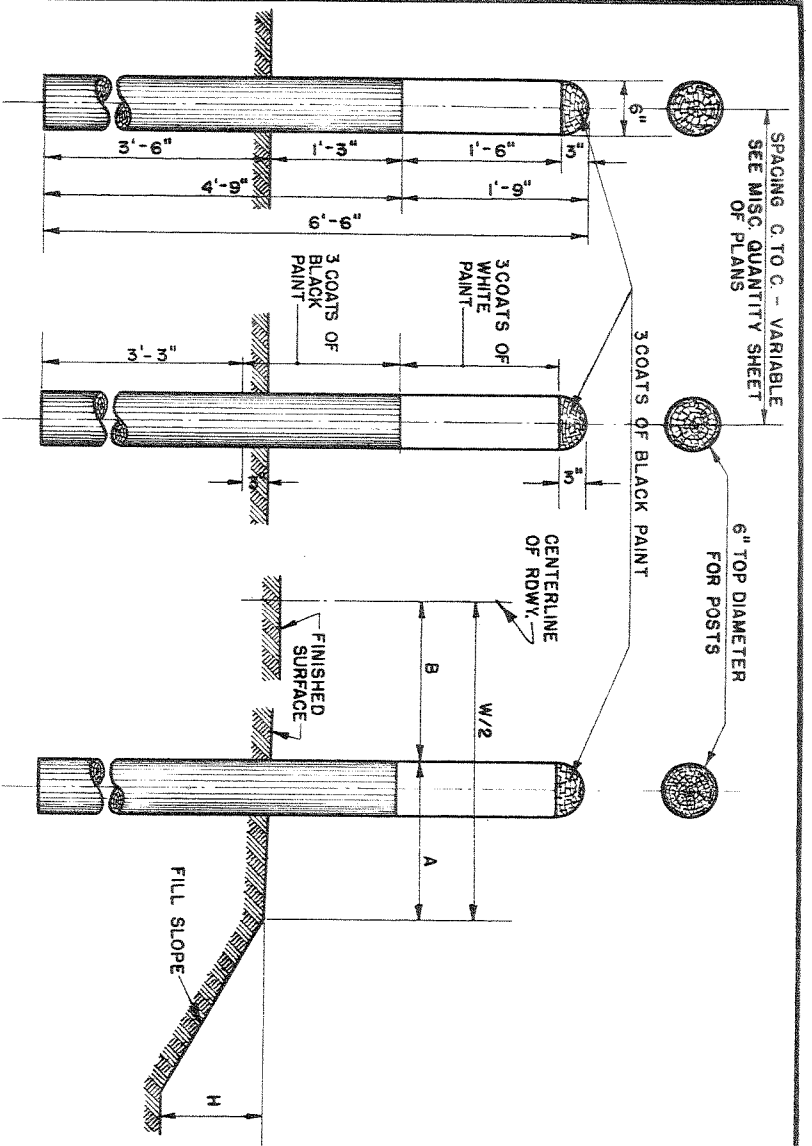
RECOMMENDED FOR APPROVAL  
*James B. Beck*  
 DESIGN ENGINEER

APPROVED  
*W. L. Beck*  
 DATE 4/20/37 STATE HIGHWAY ENGINEER

CONSULTATION ENGINEER  
*E. L. Roberts*

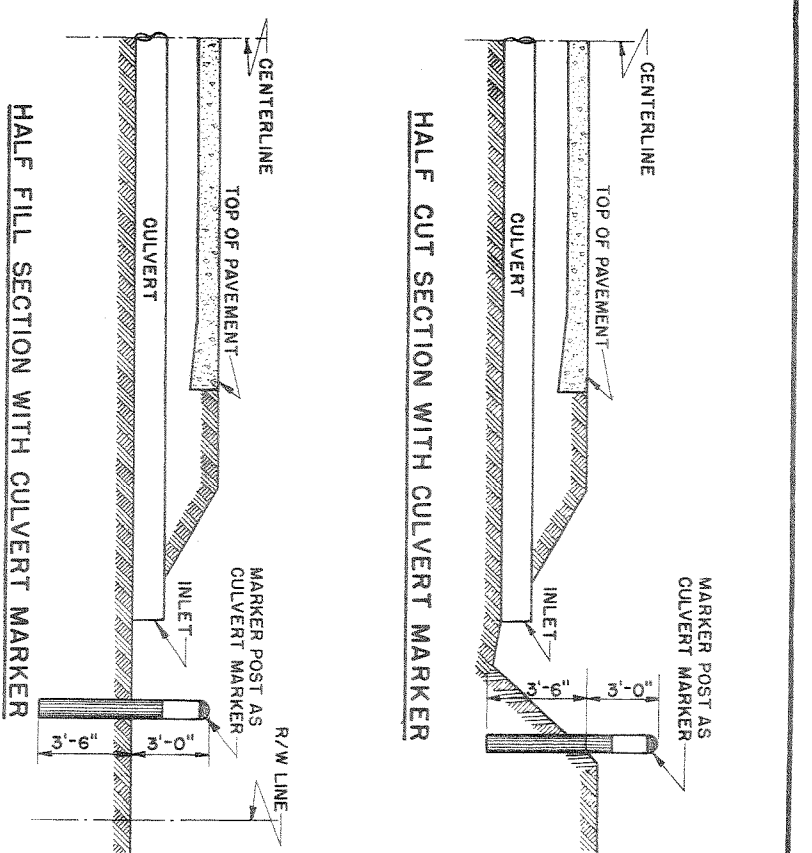
DESIGN TESTED  
 CHECKED BY *W. L. Beck*





**ELEVATION SHOWING SPACING**

**ELEVATION SHOWING LOCATION ON ROAD SHOULDER**



**HALF CUT SECTION WITH CULVERT MARKER**

**HALF FILL SECTION WITH CULVERT MARKER**

NOTE:  
MARKER POST AS CULVERT MARKER TO BE PLACED AT INLET END OF CULVERT. PLACE POST CLEAR OF CHANNEL. IN CUT, PLACE POST ON BACK SLOPE. IN FILL, PLACE POST BEYOND END OF CULVERT SO IT MAY BE VISIBLE FROM THE ROADWAY. MARKER POSTS FOR RIGHT OF WAY MARKERS SHALL BE PLACED WHERE DIRECTED BY THE ENGINEER.

**TABLE OF DIMENSIONS**

HEIGHT OF FILL	30' RDWY.			36' RDWY.			40' RDWY.		
	B	A	W/2	B	A	W/2	B	A	W/2
UNDER 5 FT.	15'	2'-6"	17'-6"	18'	2'-6"	20'-6"	20'	2'-6"	22'-6"
5 FT. TO 10'	15'	3'-6"	18'-6"	18'	3'-6"	21'-6"	20'	3'-6"	23'-6"
OVER 10 FT.	15'	4'-6"	19'-6"	18'	4'-6"	22'-6"	20'	4'-6"	24'-6"

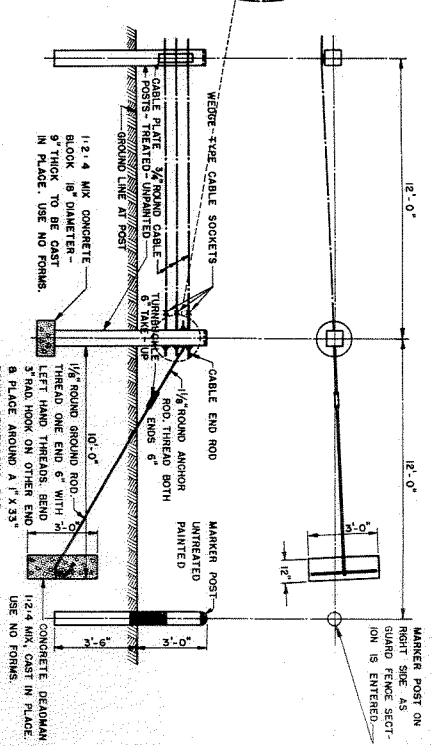
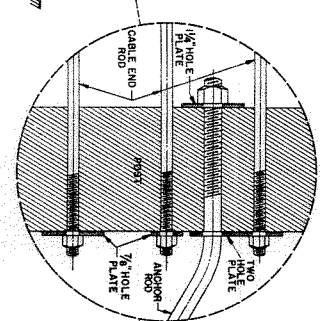
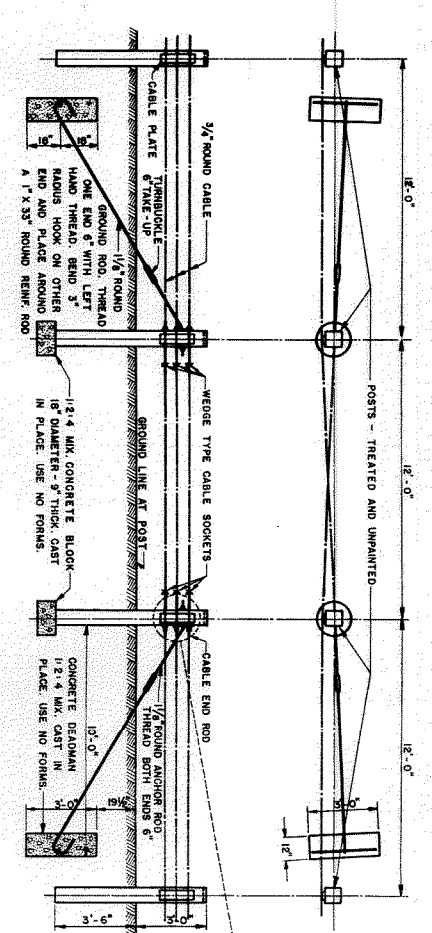
**GENERAL NOTES**

TOP OF POST SHALL BE NEATLY ROUNDED.  
POSTS TO BE CUT AND MANUFACTURED FROM LIVE, GREEN, GROWING NORTHERN WHITE CEDAR, YELLOW PINE, NORWAY PINE, OR WHITE PINE, STACKED & SEASONED IN AN APPROVED MANNER.  
PAINT AND APPLICATION SHALL CONFORM TO PERTINENT SPECS.  
PORTION OF POST TO BE PAINTED SHALL BE SHAVED TO THE WHITE BEFORE PAINTING.

STATE HIGHWAY COMMISSION OF WISC.  
RECOMMENDED FOR APPROVAL:

APPROVED:  
DRAWN: TBS  
CHECKED: N.F.C.  
DATE: 3/10/37  
DESIGN ENGINEER: [Signature]  
CONSTRUCTION ENGINEER: [Signature]

**MARKER POSTS**

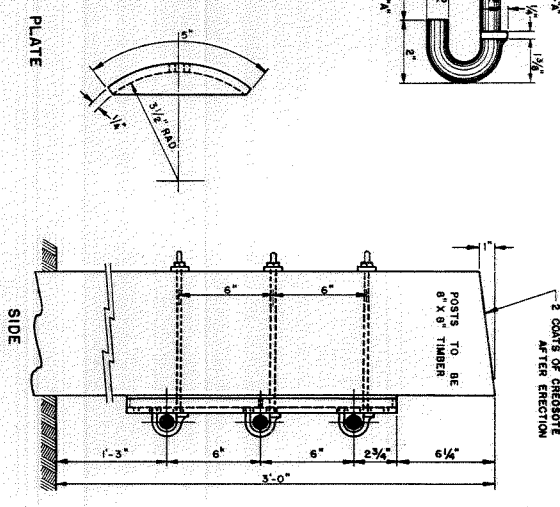
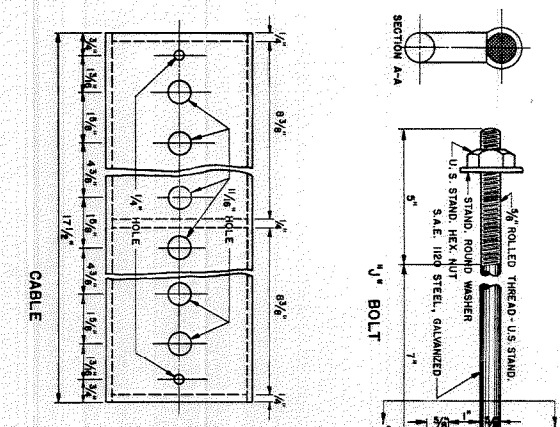
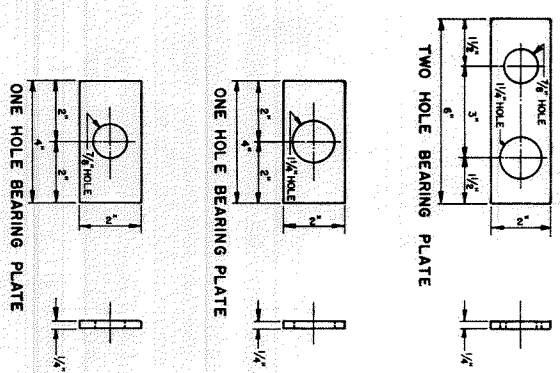


INTERMEDIATE ANCHOR

END ANCHOR

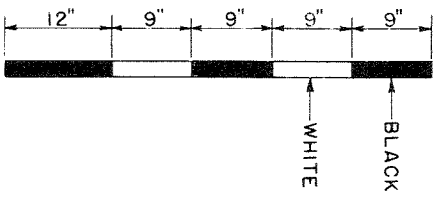
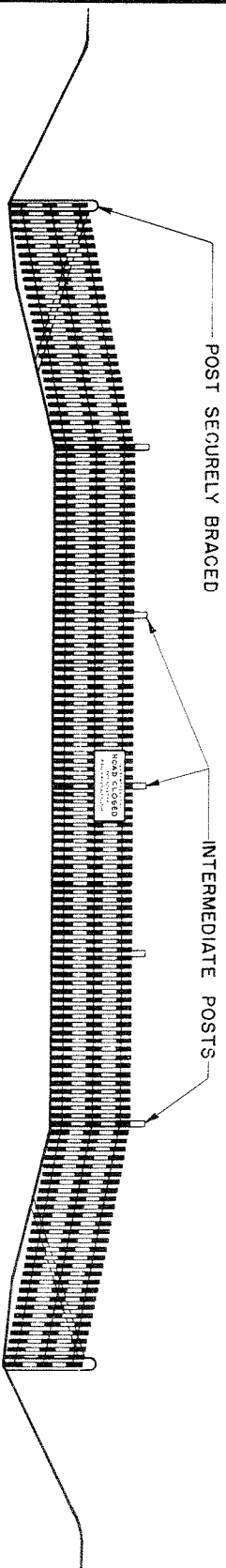
GENERAL CONSTRUCTION NOTES

DETAILS OF CONSTRUCTION NOT COVERED ON THIS SHEET SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION. FOUR DETAILED DRAWINGS OF SOCKETS, TURNBUCKLES AND PLATES MUST BE SUBMITTED TO THE STATE HIGHWAY COMMISSION FOR APPROVAL BEFORE THEIR USE WILL BE PERMITTED. ALL FITTINGS TO BE MALLEABLE IRON CASTINGS. ALL PLATES TO BE GALVANIZED AND BREAKAGE OF GALVANIZING DURING ERECTION WILL BE CAUSE FOR REJECTION, AND MUST BE REPLACED. AFTER REJECTION, ALL CABLE END BOLTS AND "J" BOLTS PROJECTING MORE THAN ONE INCH FROM NUT SHALL BE CUT OFF ONE HALF INCH FROM NUT AND CHECKED. VARIATIONS FROM THESE DESIGNS OF FITTINGS WILL BE CONSIDERED UPON SUBMISSION OF FOUR COPIES OF DETAILED PLANS FOR APPROVAL. IN GENERAL, NO CONTINUOUS SECTION OF CABLE SHALL EXCEED 750 FEET WITHOUT INTERMEDIATE ANCHORAGE. SEE TYPICAL SECTION SHEET FOR LOCATION OF RAIL IN REFERENCE TO SHOULDER.



STANDARD DESIGN FOR  
**3 WIRE CABLE GUARD FENCE**  
 STATE HIGHWAY COMMISSION OF WISCONSIN  
 RECOMMENDED FOR APPROVAL  
 APPROVED DATE 3/10 1937  
 STATE ENGINEER  
 DRAWN BY  
 CHECKED BY

SECTION OF SNOW FENCE TO BE PAINTED ALTERNATELY BLACK AND WHITE AS SHOWN BELOW. LENGTH TO BE SUFFICIENT TO REACH FROM TOE OF THE BACKSLOPE TO TOE OF THE BACKSLOPE. PROVIDE SUFFICIENT NUMBER OF INTERMEDIATE POSTS TO ADEQUATELY SUPPORT THE FENCE. STANDARD "ROAD CLOSED" SIGN (W.H.C. NO. R-6) TO BE ERRECTED ON FENCE AT THE TOP ON THE CENTERLINE OF ROAD.



DETAIL SHOWING METHOD OF STRIPING FOR 4' SNOW FENCE.

STATE HIGHWAY COMMISSION OF WISCONSIN  
**CONSTRUCTION BARRICADE**

RECOMMENDED FOR APPROVAL:  
*Frank Green*  
 DESIGN ENGINEER  
*W. J. Miller*  
 CONSTRUCTION ENGINEER  
 DATE 6/13/35

APPROVED:  
*E. G. Rottgen*  
 STATE HIGHWAY COMMISSION ENGINEER  
 DRAWN H.M. DATE 6/13/35  
 CHECKED W.H.G.