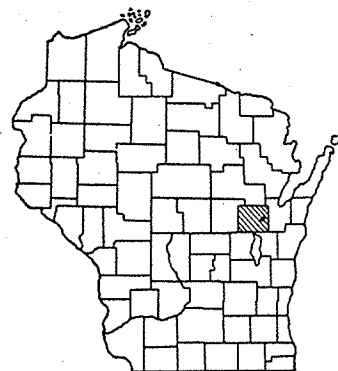


INDEX OF SHEETS

- SHEET NO. 1 TITLE
- SHEET NO. 2 TYPICAL CROSS SECTIONS
- SHEET NO. 2 ESTIMATE OF QUANTITIES
- SHEET NO. 2 MISCELLANEOUS QUANTITIES
- SHEET NO. — RIGHT OF WAY PLAT
- SHEET NO. 3-11 PLAN AND PROFILE STA. 42+00 TO STA. 290+04.75
- SHEET NO. 12-16 STANDARD DETAILS
- SHEET NO. — DRAINAGE STRUCTURES
- SHEET NO. 17-37 CROSS SECTIONS



STATE OF WISCONSIN
STATE HIGHWAY COMMISSION OF WISCONSIN

PLAN AND PROFILE OF PROPOSED
FREEDOM — C.T.H. "EE" ROAD
C.T.H. "E"
OUTAGAMIE COUNTY PROJECT S 0377 (2)

COUNTY AND HIGHWAY	ROUTE AND SECTION	CLASS AND AGREEMENT		S.F.R. REGION DIVISION	SHEET NUMBER	TOTAL SHEETS
		STATE	FEDERAL			
44.6	377.0		11.2	4 WIS.	1	37

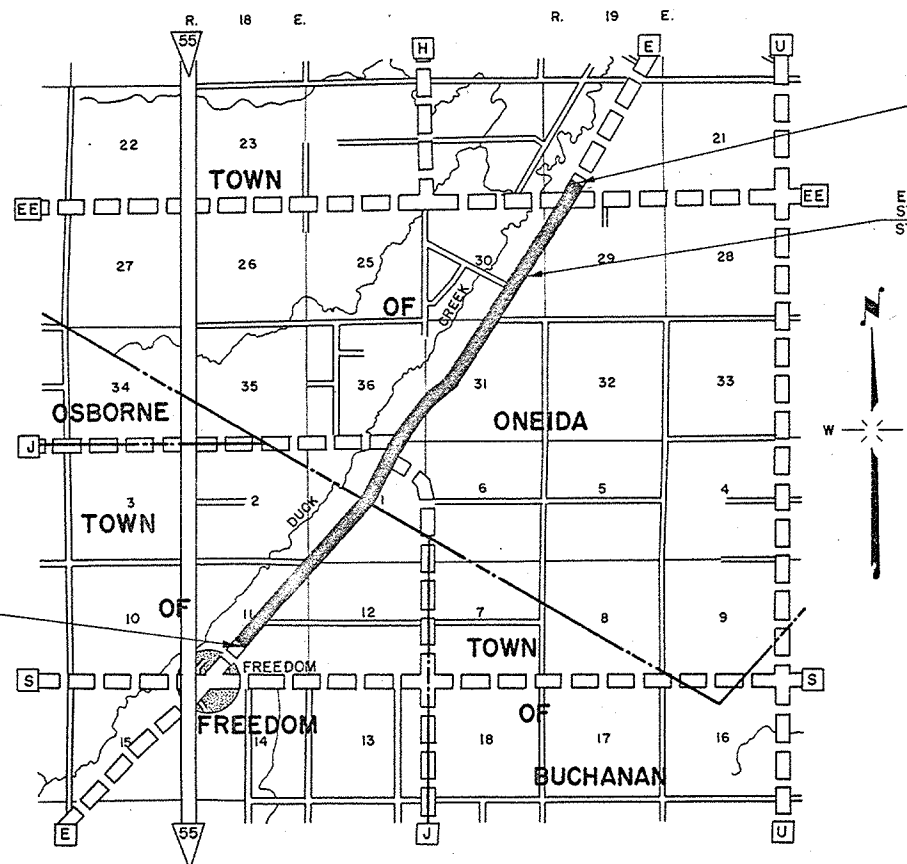
9/30/63

PLAN 1 IN. = 100 FT.
PROFILE HOR. 1 IN. = 100 FT. VERT. 1 IN. = 10 FT.
CROSS SECTIONS HOR. 1 IN. = 5 FT. VERT. 1 IN. = 5 FT.

STA. 42+00 BEG. OF PROJECT S 0377 (2)
962 SO. & 415' WEST OF THE CTR. OF SEC. 11, T22N, R.18E.

STA. 290+04.75 END OF PROJECT S 0377 (2)
307 NO. & 1,443' WEST OF THE S.E. COR. OF THE SW 1/4 OF SEC. 20, T23N, R.19E.

EQUATION:
STA. 245+00.8 BACK =
STA. 245+02.3 AHEAD



LAYOUT
SCALE ONE MILE

TOTAL NET LENGTH OF CENTERLINE = 4.698 MI.

CONVENTIONAL SIGNS

- | | |
|---|---|
| <ul style="list-style-type: none"> STATE LINE..... COUNTY LINE..... TOWNSHIP OR RANGE LINE..... SECTION LINE..... NEW RIGHT OF WAY LINE..... PRESENT RIGHT OF WAY LINE..... WIRE FENCE { WOVEN..... <li style="padding-left: 20px;">BARBED..... LOT LINE..... CORPORATE OR CITY LIMITS..... PROPERTY LINE..... TRAVELED WAY OR P.E..... RAILROADS..... BASE OR SURVEY LINE..... | <ul style="list-style-type: none"> CULVERTS IN PLACE..... CULVERTS REQUIRED..... DROP INLET..... POWER POLE..... TELEPHONE OR TELEGRAPH POLE..... RIGHT OF WAY MARKERS..... REFERENCE STAKE FOR HUBS ONLY..... MARSH..... HEDGE..... TREES..... GROUND ELEVATION..... DATUM LINE 73.9 GRADE ELEVATION..... DATUM LINE 75.16 |
|---|---|

County Project #113

E-7

STATE HIGHWAY COMMISSION OF WISCONSIN MADISON, WIS.	
SURVEYOR D.G.N.	NOTE BOOK 438 & 439
DISTRICT COMPUTER R.L.V.	M. O. CHECKER W.H.B.
DISTRICT CHECKER R.H.V.	CORRECT
CORRECT	
DATE 8/28/63	<i>[Signature]</i> DISTRICT ENGINEER
RECOMMENDED FOR APPROVAL:	
DATE 9/4/63	<i>[Signature]</i> ENGINEER OF DESIGN
APPROVED:	
DATE 9/1/63	<i>[Signature]</i> STATE HIGHWAY COMMISSIONER
DEPARTMENT OF COMMERCE BUREAU OF PUBLIC ROADS	
APPROVED:	
DATE	
DIVISION ENGINEER	

E-7

S 0377 (2)

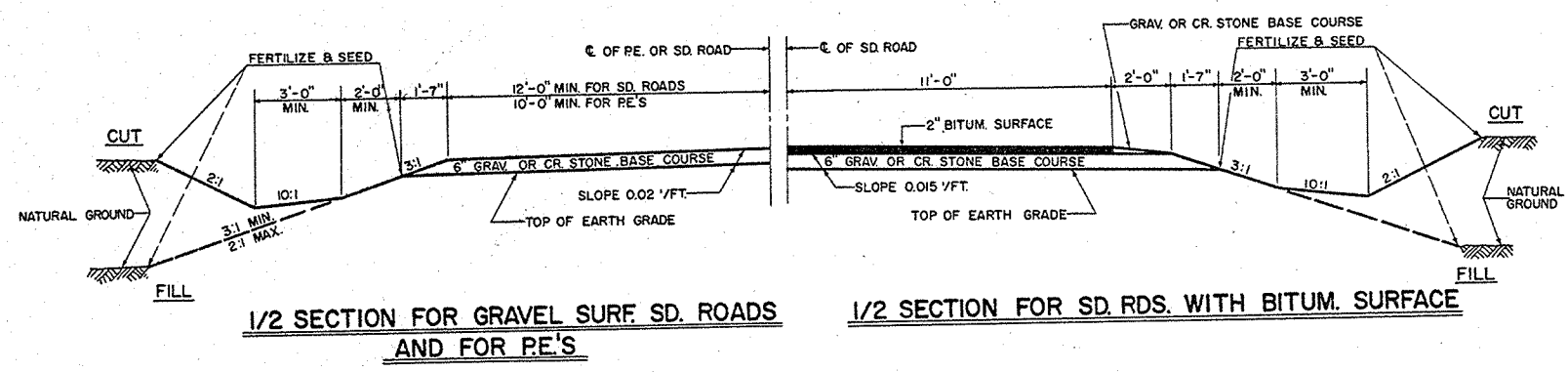
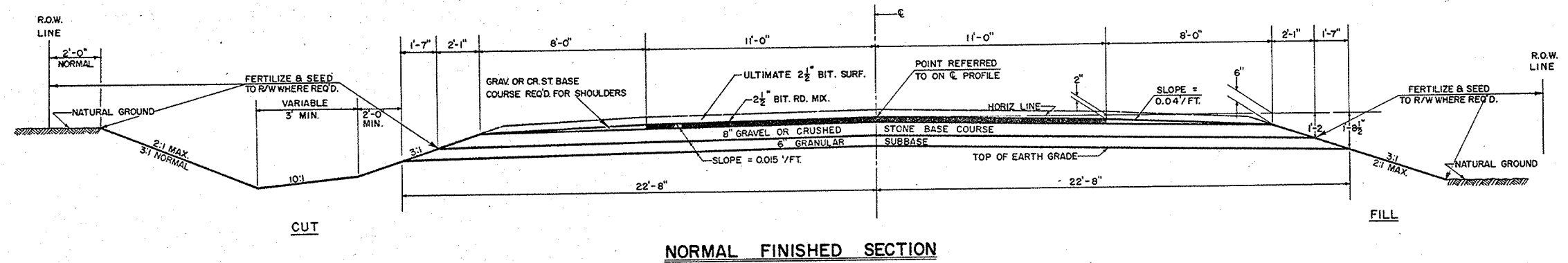
ESTIMATE OF QUANTITIES

CONTRACT NO. 182

PROJECT	SHEET NUMBER	TOTAL SHEETS
S 0377(2)	2	37

THIS PROJECT IS TO BE EXECUTED UNDER THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION OF THE STATE HIGHWAY COMMISSION OF WISCONSIN—EDITION OF 1963
SUBMITTED FOR APPROVAL JANUARY 25, 1963, AND SPECIAL PROVISIONS AS ATTACHED TO PROPOSAL.

CONTRACT NO.	STATION TO STATION	NET LENGTH OF CENTER LINE	EXCAVATION			GRANULAR SUB-BASE COURSE	FINISHING ROADWAY	GRAVEL OR CRUSHED STONE BASE COURSE	SHAPING AND COMPACTING GRAVEL OR CRUSHED STONE BASE COURSE	CLASS III - CULVERT PIPE			MORTAR RUBBLE DITCH CHECKS	GUARD FENCE	MARKER POSTS FOR R/W	LAND-MARK REFER. MONU-MENTS	CALCIUM CHLORIDE SURF. TREAT-MENT	TOPSOIL		FER-TIL-IZER	SEEDING				
			UNCLASSIFIED	BORROW						18"	24"	36"													
			C.Y.	C.Y.	C.Y.					LS.	TON	TON						TON	TON			TON	C.Y.	LN. FT.	EACH
1	42+00 - 290+04.75	24,803.25	1,162	1,162	1	48,083	6,700	23,300	1			52,800	2,066	1,402	64	110		56			15	33	101,680	120	132,800
2	42+00 - 290+04.75	24,803.25										52,800													
TOTALS		24,803.25	1,162	1,162	1	48,083	6,700	23,300	1			52,800	2,066	1,402	64	110		56			15	33	101,680	120	132,800



CULVERT PIPE - CLASS III			
STATION	LOCATION	DIA.	LN. FT.
53+03	CL	24"	68
55+10	SD. RD. RT.	18"	66
57+20	CL	24"	64
66+96	CL	24"	84
87+61	CL	24"	58
91+97	CL	24"	76
99+21	CL	24"	60
104+76	CL	24"	54
108+62	CL	24"	72
112+55	CL	24"	70
120+13	CL	24"	86
130+55	CL	24"	66
136+14	CL	24"	70
158+74	CL	24"	58
168+47	CL	24"	60
178+01	CL	24"	68
192+83	CL	24"	94
206+67	CL	24"	68
223+72	CL	24"	80
261+36	CL	24"	60
265+40	PELT.	18"	48
276+26	CL	36"	64
287+59	CL	24"	54
89+16	P.E. RT.	24"	32
61 P.E. PIPES - 18" X 32'			1,952

MARKER POSTS	
LOCATION	EACH
FOR CULVERT PIPES:	44
FOR SHOULDER DELINEATION: STA. 190+00-195+00 LT. & RT.	12

GRAV. OR CR. STONE BASE COURSE	
LOCATION	TON
MAINLINE	49,400
SHOULDER	
SIDE ROADS & P.E.'S	3,400

ABANDONING WELLS	
LOCATION	EACH
189+06 LT.	1

GENERAL NOTES

CONSTRUCTION DETAILS NOT SHOWN HERE SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

WHEN THE QUANTITY OF THE ITEMS OF SUBBASE, BASE OR SURFACE COURSE IS MEASURED FOR PAYMENT BY THE TON OR CUBIC YARD, THE DEPTH OR THICKNESS OF THE COURSE SHOWN ON THE PLANS IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL DIRECTED BY THE ENGINEER.

ON ALL HORIZONTAL CURVES HAVING A SUPER-ELEVATION, THE NORMAL CROWN SHALL BE REMOVED AND THE PAVEMENT SURFACE, BASE COURSE, AND EARTH GRADE SHALL RESULT IN A STRAIGHT LINE SLOPE ACROSS THE ENTIRE WIDTH OF ROADBED.

SALVAGED TOPSOIL TO BE PLACED ON ALL CUT SLOPES AND ALL FILL SLOPES TO AN APPROXIMATE DEPTH OF 3" AT TIME OF PLACING.

BITUMINOUS SURFACING NOT PART OF THIS PROJECT

STANDARD DETAIL DRAWINGS

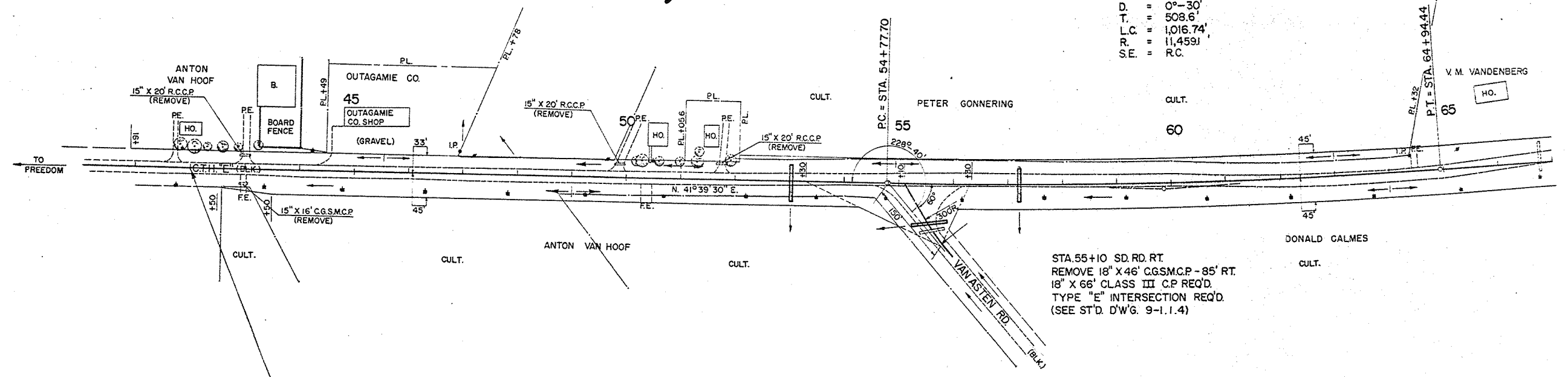
MARKER POSTS	7- 1.3.4
CONSTRUCTION BARRICADE	7- 4.1.4
DITCH CHECKS, M.R.M. & SOD	8- 1.3.1
DESIGN & LAYOUT DETAILS FOR SD. RDS.	9- 1.1.4
LANDMARK REFERENCE MONUMENTS	12- 1.1.2

TYPICAL CROSS SECTION
FOR
22' BITUMINOUS SURFACE
AND
MISCELLANEOUS CONSTRUCTION DETAILS
SCALE VARIABLE

BENCH MARKS				
NO.	STATION	DESCRIPTION	ELEV.	
7	41+89	PT. MK. COR. BOT. STEP 60' LT.	749.28	
8	45+95	PT. MK. N.E. COR. CONC. SLAB 80' LT.	748.54	
9	51+30	SPIKE IN BOX ELDER 45' LT.	751.56	
10	65+28	" " POWER POLE 40' RT.	754.17	

D.P.N. DIVISION	PROJECT	SHEET NUMBER	TOTAL SHEETS
4	S 0377 (2)	3	37

ARC DEF. CURVE NOTES
 P.I. = 59+86.3
 L = 174'-55"
 Δ = 5°-05'
 D = 0°-30'
 T = 508.6'
 L.C. = 1,016.74'
 R = 11,459.1'
 S.E. = R.C.

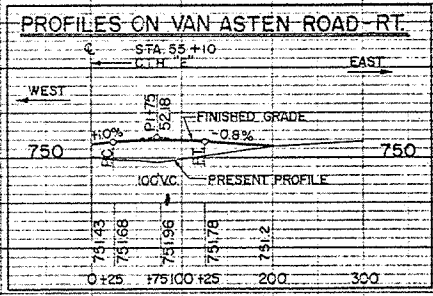


NET LENGTH OF CENTERLINE		
STATION TO STATION	LIN. FT.	
42+00	65+00	2,300.00

STA. 42+00 BEG. OF PROJ. S 0377 (2)

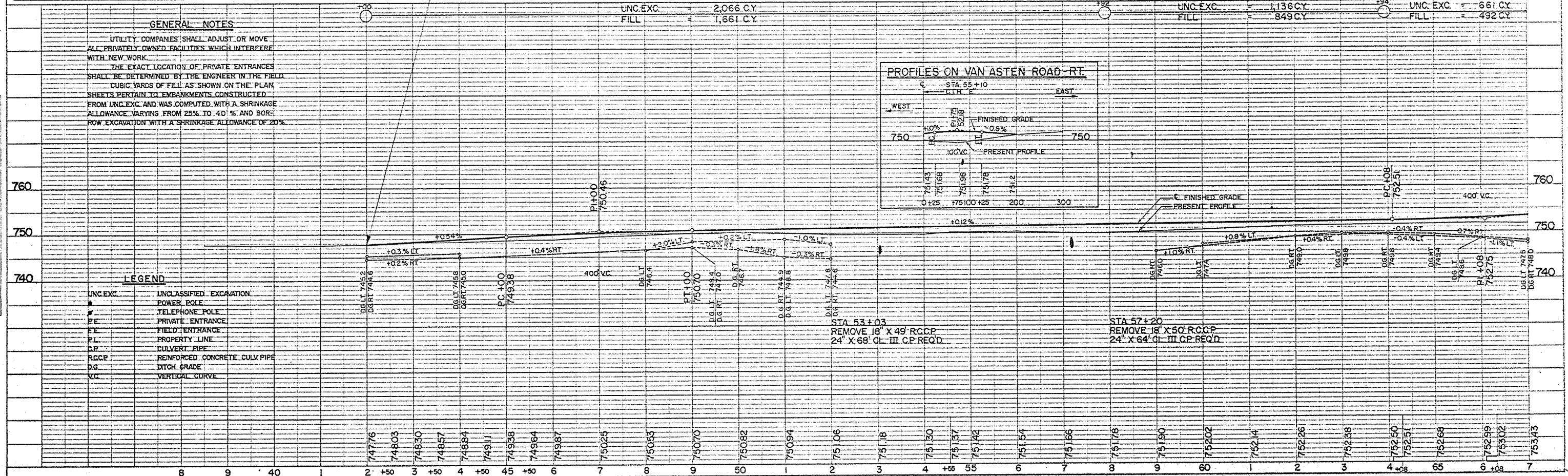
GENERAL NOTES
 UTILITY COMPANIES SHALL ADJUST OR MOVE ALL PRIVATELY OWNED FACILITIES WHICH INTERFERE WITH NEW WORK.
 THE EXACT LOCATION OF PRIVATE ENTRANCES SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.
 CUBIC YARDS OF FILL AS SHOWN ON THE PLAN SHEETS PERTAIN TO EMBANKMENTS CONSTRUCTED FROM UNC. EXC. AND WAS COMPUTED WITH A SHRINKAGE ALLOWANCE VARYING FROM 25% TO 40% AND BORROW EXCAVATION WITH A SHRINKAGE ALLOWANCE OF 20%.

UNC. EXC.	2,066 C.Y.	UNC. EXC.	1,136 C.Y.	UNC. EXC.	661 C.Y.
FILL	1,661 C.Y.	FILL	849 C.Y.	FILL	492 C.Y.



LEGEND

UNC. EXC.	UNCLASSIFIED EXCAVATION
▲	POWER POLE
○	TELEPHONE POLE
PE	PRIVATE ENTRANCE
FE	FIELD ENTRANCE
PL	PROPERTY LINE
C.P.	CULVERT PIPE
R.C.C.P.	REINFORCED CONCRETE CULVERT PIPE
D.G.	DITCH GRADE
VC	VERTICAL CURVE



STA 53+03
 REMOVE 18" X 49" R.C.C.P.
 24" X 66" CL III C.P. REQ'D.

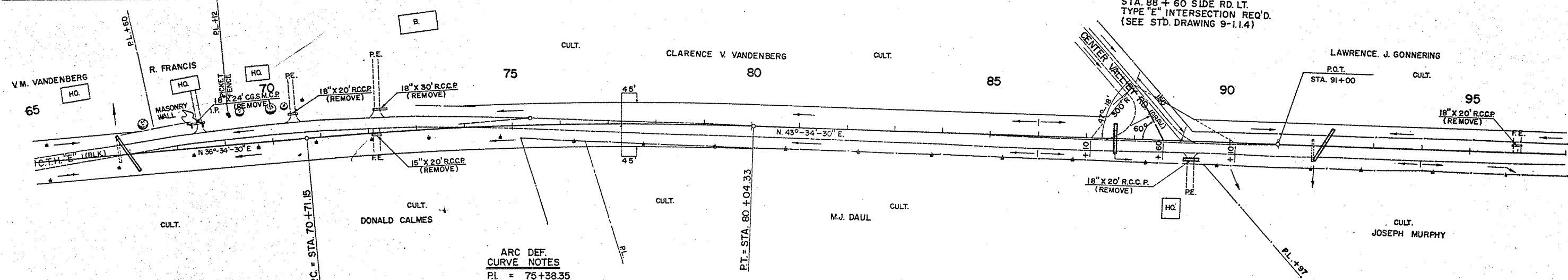
STA 57+20
 REMOVE 18" X 50" R.C.C.P.
 24" X 64" CL III C.P. REQ'D.

PLAN
 5-62
 D.S.N.
 438

PROFILE
 5-62
 D.S.N.
 438

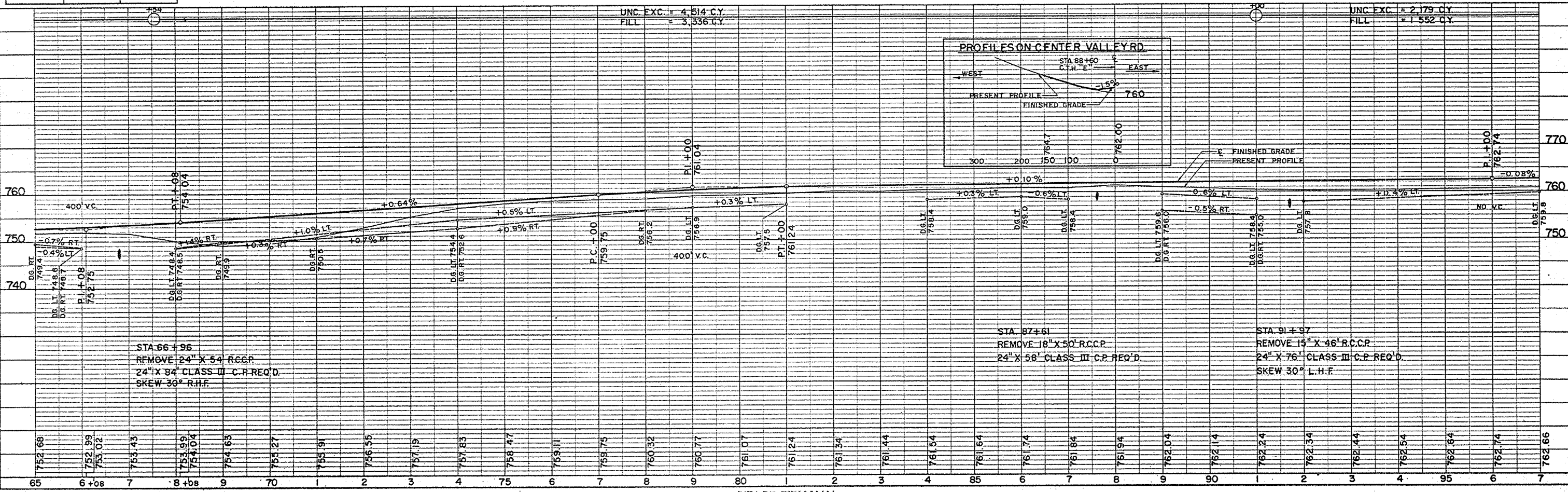
BENCH MARKS			
NO.	STATION	DESCRIPTION	ELEV.
10	65+28	SPIKE IN POWER POLE 40' RT.	754.17
11	70+03	" " BOX ELDER 67' RT.	753.42
12	77+75	" " POWER POLE 37' RT.	757.88
13	83+75	" " " " 45' RT.	761.43
14	89+10	" " 8" CEDAR 93' RT.	759.54
15	95+75	" " POWER POLE 45' RT.	757.29

B. P. R. DIVISION	PROJECT	SHEET NUMBER	TOTAL SHEETS
4	S 0377 (2)	4	37



ARC DEF. CURVE NOTES
 PI = 75+38.35
 L = 187°-00'
 Δ = 7°-00'
 D = 0°-45'
 T = 467.2'
 L.C. = 933.18
 R = 7639.44'
 S.E. = 0.018'/FT.

NET LENGTH OF CENTERLINE		
STATION TO STATION	LIN. FT.	
65+00	95+00	3000.00

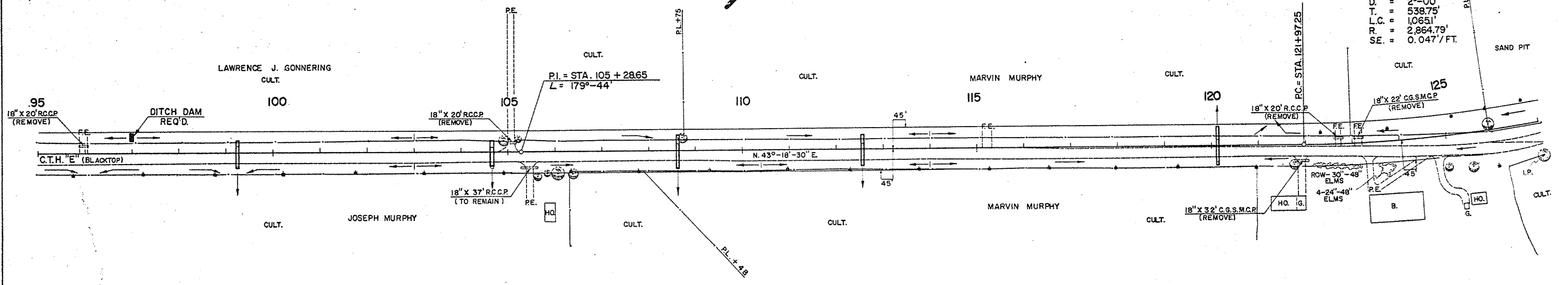


BENCH MARKS

NO.	STATION	DESCRIPTION	ELEV.
15	95+75	SPIKE IN POWER POLE 45' RT.	757.29
16	105+60	" " 18" BOX. ELDER 50' RT.	760.80
17	112+33	" " POWER POLE 45' RT.	750.75
18	122+60	" " 24" ELM 43' RT.	751.53

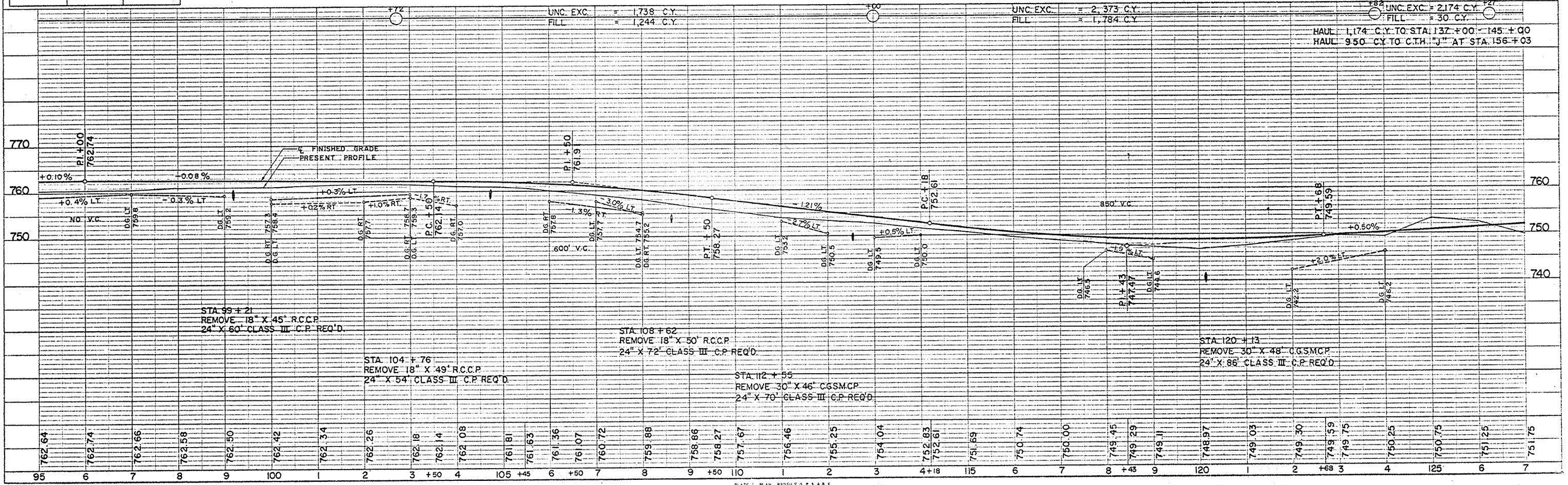
ARC. DEF. CURVE NOTES

PI = 127+36.0
 L = 158°-42'
 Δ = 21°-18'
 D = 2°-00'
 T = 538.75'
 L.C. = 1,065.1'
 R = 2,864.79'
 S.E. = 0.047'/FT.



NET LENGTH OF CENTERLINE

STATION TO STATION	LIN. FT.	
95+00	125+00	3,000.00



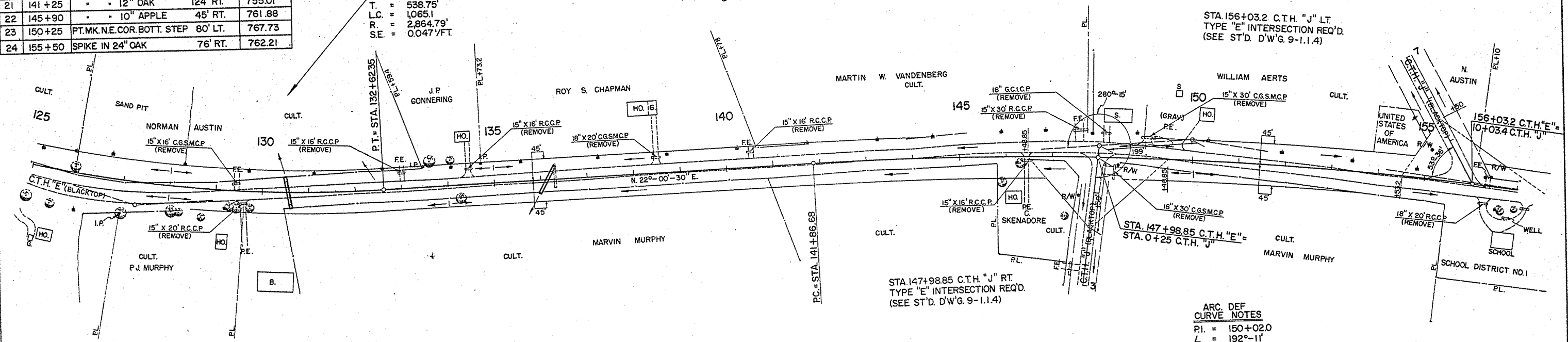
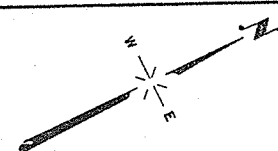
PLAN
 DRAWN BY: R.W.V.
 CHECKED BY: R.W.V.
 NO. 438

PROFILE
 DRAWN BY: R.W.V.
 CHECKED BY: R.W.V.
 NO. 439

U.P.R. DIVISION	PROJECT	SHEET NUMBER	TOTAL SHEETS
4	S 0377(2)	6	37

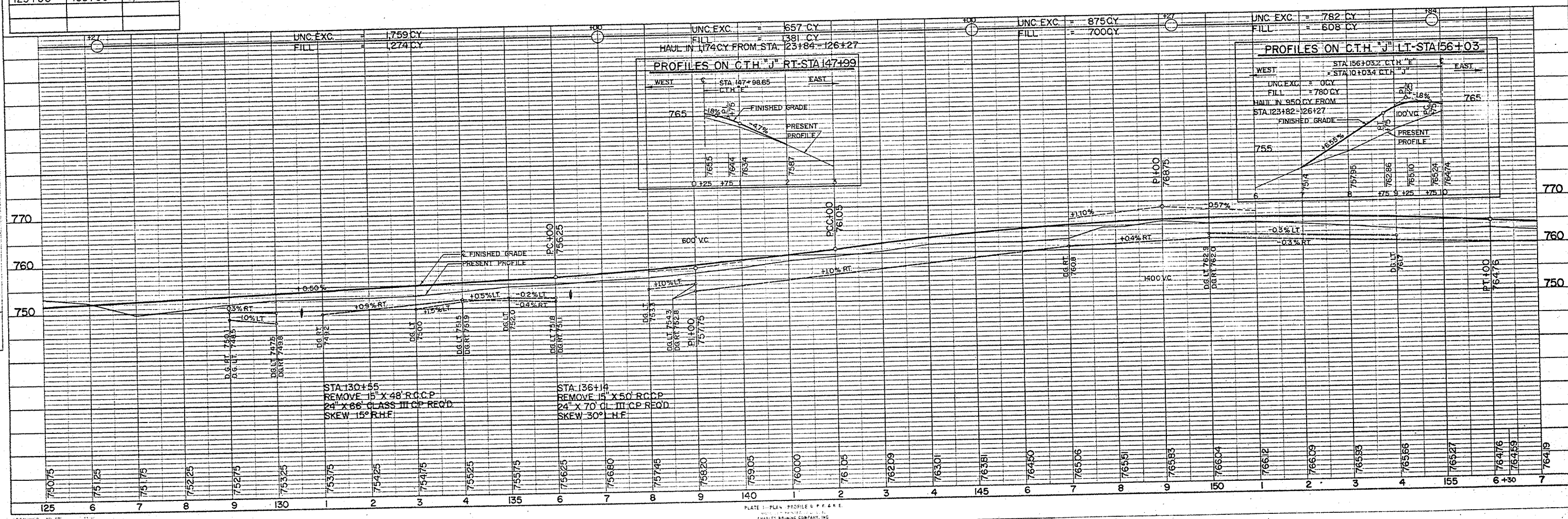
BENCH MARKS			
NO.	STATION	DESCRIPTION	ELEV.
19	129+75	SPIKE IN 36" ELM 35' RT.	752.77
20	134+05	" 12" POPLAR 54' LT.	756.48
21	141+25	" 12" OAK 124' RT.	755.01
22	145+90	" 10" APPLE 45' RT.	761.88
23	150+25	PT.MK.N.E.COR.BOTT. STEP 80' LT.	767.73
24	155+50	SPIKE IN 24" OAK 76' RT.	762.21

ARC. DEF. CURVE NOTES
 P.I. = 127+36.0
 L = 158'-42'
 Δ = 2°-18'
 D. = 2°-00'
 T. = 538.75'
 L.C. = 1065.1
 R. = 2,864.79'
 S.E. = 0.047 V/FT.



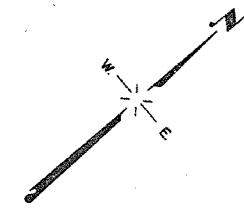
ARC. DEF. CURVE NOTES
 P.I. = 150+02.0
 L = 192'-11'
 Δ = 12°-11'
 D. = 0°-45'
 T. = 815.32'
 L.C. = 1,624.6'
 R. = 7,639.44'
 S.E. = 0.018 V/FT.

NET LENGTH OF CENTERLINE		
STATION TO STATION	LIN. FT.	
125+00	155+00	3,000.00

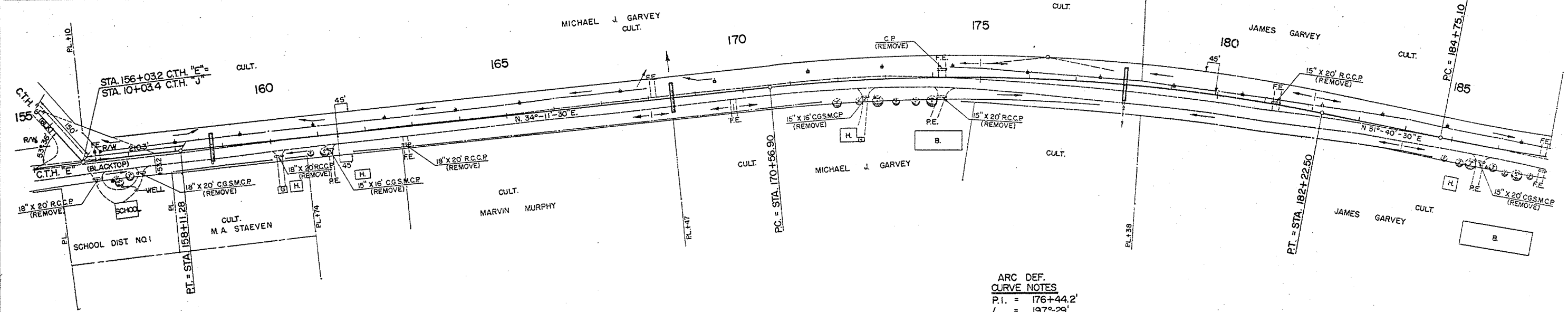


BENCH MARKS

NO.	STATION	DESCRIPTION	ELEV.
24	155+50	SPIKE IN 24" OAK	76' RT. 762.21
25	160+95	" " 8" TWIN POPLAR	62' RT. 760.76
26	168+45	" " 12" MAPLE	67' RT. 764.22
27	173+97	" " 18" ELM	50' RT. 764.60
28	178+29	" " 48" "	85' LT. 767.76
29	185+25	" " 10" MAPLE	40' RT. 769.25



PLAN
 DRAWN BY: [Name]
 CHECKED BY: [Name]
 DATE: [Date]
 NO. 438

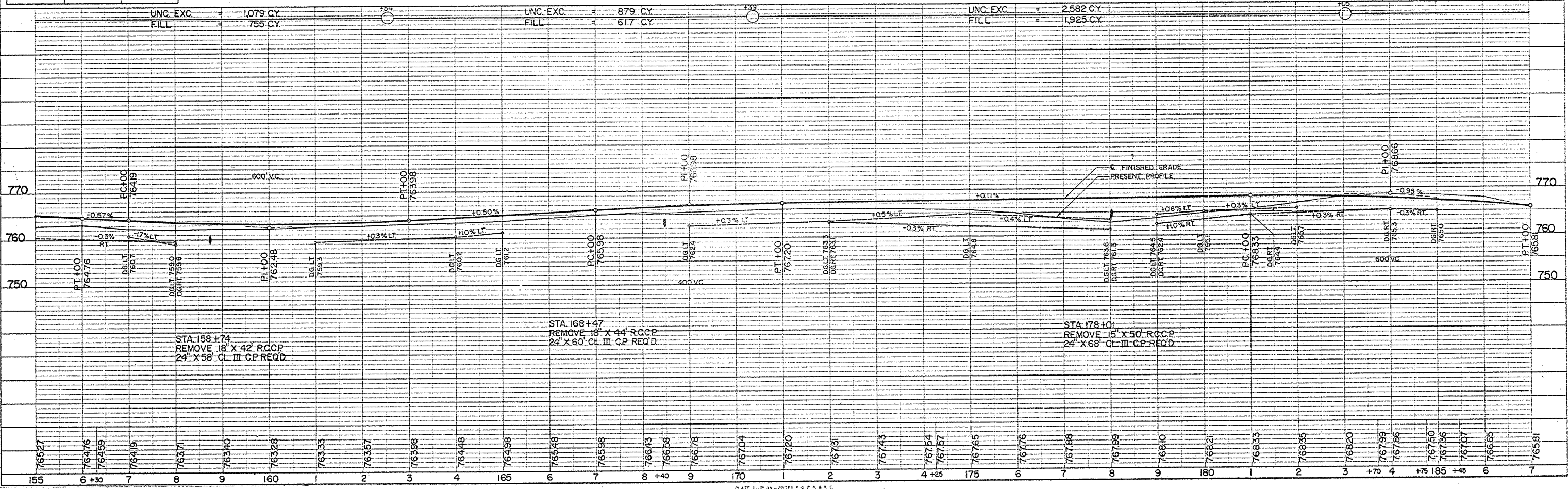


NET LENGTH OF CENTERLINE

STATION TO STATION	LIN. FT.	
155+00	185+00	3,000.00

ARC DEF. CURVE NOTES
 P.I. = 176+44.2'
 L = 197°-29'
 Δ = 17°-29'
 D = 1°-30'
 T = 587.3'
 L.C. = 1,165.6'
 R = 3,819.71'
 S.E. = 0.035'/FT

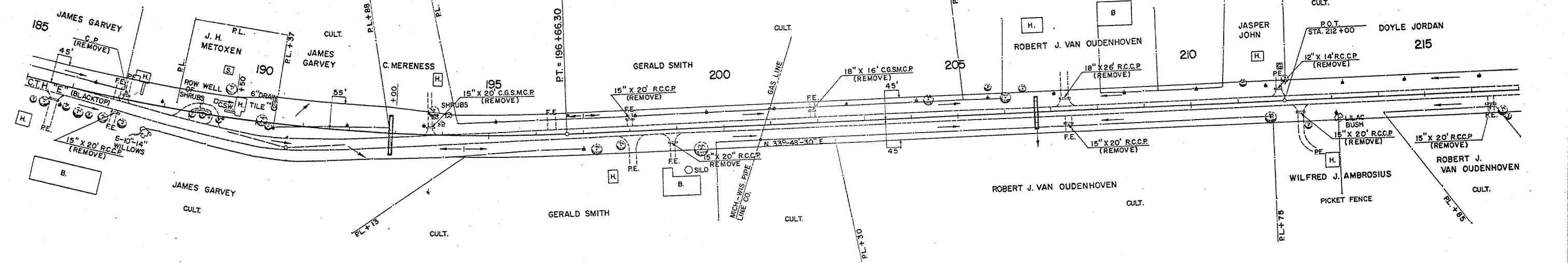
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 NO. 439



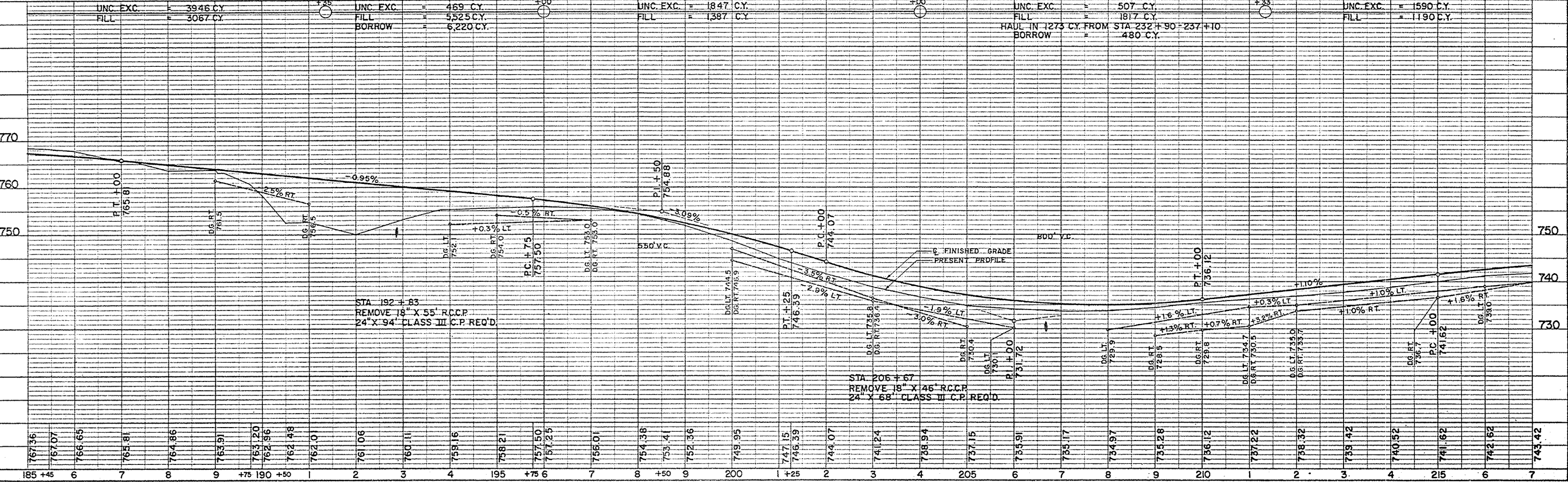
BENCH MARKS			
NO	STATION	DESCRIPTION	ELEV.
29	185+25	SPIKE IN 10" MAPLE	40' RT. 769.25
30	190+60	" " 10" B. ELDER	68' RT. 762.55
31	193+90	" " " " "	50' RT. 755.42
32	199+45	" " 12" " "	48' RT. 755.69
33	206+40	" " 8" POPLAR	107' LT. 733.90
34	212+50	" " 18" ELM	55' RT. 735.34
35	216+75	" " 30" POPLAR	54' RT. 742.43

ARC DEF. CURVE NOTES
 P.I. = 190+75.6
 L = 162°-08'
 Δ = 17°-52'
 D = 1°-30'
 T = 600.5'
 L.C. = 1,191.2'
 R = 3,819.71'
 S.E. = 0.035' / FT.

B. P. R. DIVISION	PROJECT	SHEET NUMBER	TOTAL SHEETS
4	S 0377 (2)	8	37



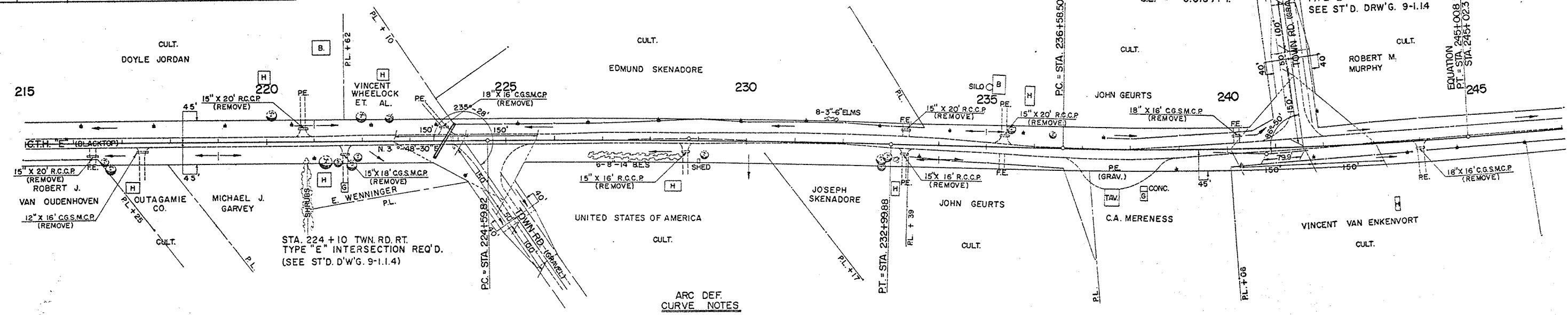
NET LENGTH OF CENTERLINE		
STATION TO STATION	LIN. FT.	
185+00	215+00	3,000.00



BENCH MARKS				
NO.	STATION	DESCRIPTION		ELEV.
35	216+75	SPIKE IN 30" POPLAR	54' RT.	742.43
36	222+00	" " 12" APPLE	50' LT.	745.01
37	227+85	" " 10" "	54' RT.	746.93
38	232+90	" " 8" POPLAR	72' RT.	763.40
39	236+75	PT. MK. NW. SIDE SIGN ANCHOR	48' RT.	762.97
40	243+12	SPIKE IN 14" APPLE	53' RT.	753.67

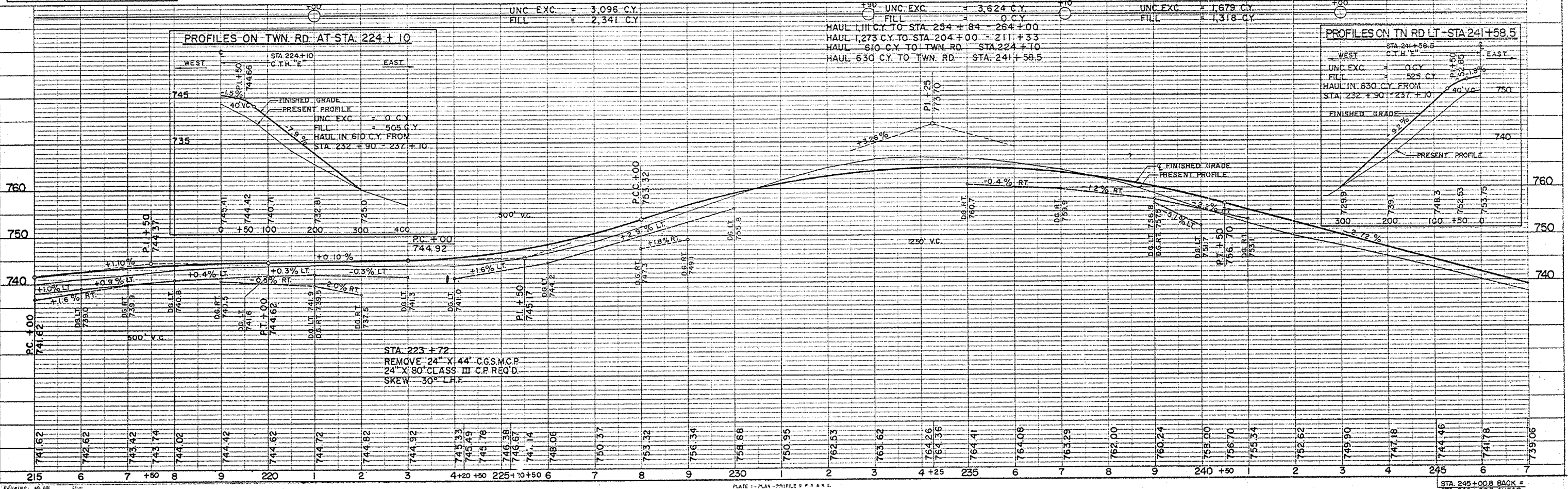
DIVISION	PROJECT	SHEET NUMBER	TOTAL SHEETS
4	S 0377 (2)	9	37

ARC DEF. CURVE NOTES
 P.I. = 240+80.1
 L = 173'-4"
 Δ = 6°-19'
 D = 0°-45'
 T = 421.6'
 L.C. = 842.3'
 P.E. = 7639.44'
 S.E. = 0.018'/FT.



NET LENGTH OF CENTERLINE		
STATION TO STATION	LIN. FT.	
215+00	245+00	3000.00

ARC DEF. CURVE NOTES
 P.I. = 228+79.9
 L = 182'-06"
 Δ = 2°-06'
 D = 0°-15'
 T = 420.08'
 L.C. = 840.06'
 R = 22,920.0'

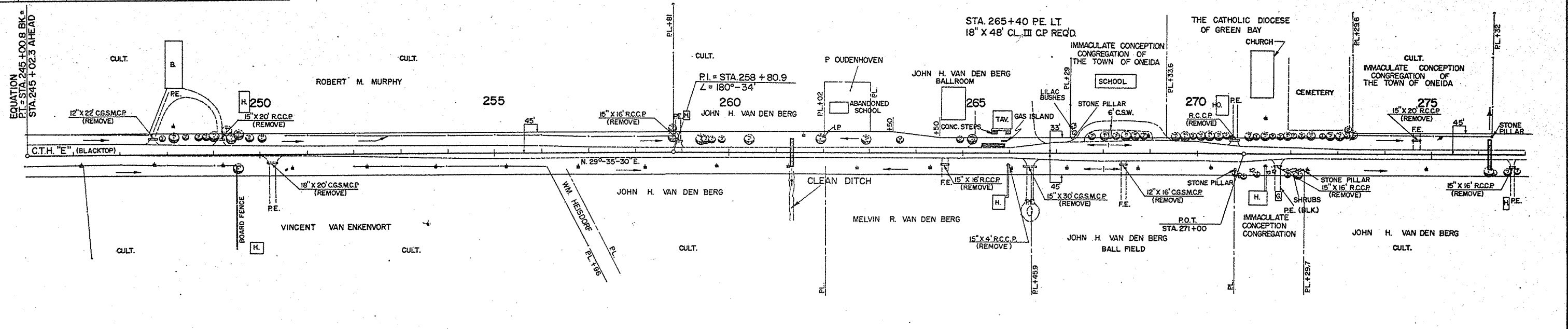


PLAN
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 DATE: [Date]
 NO. 438

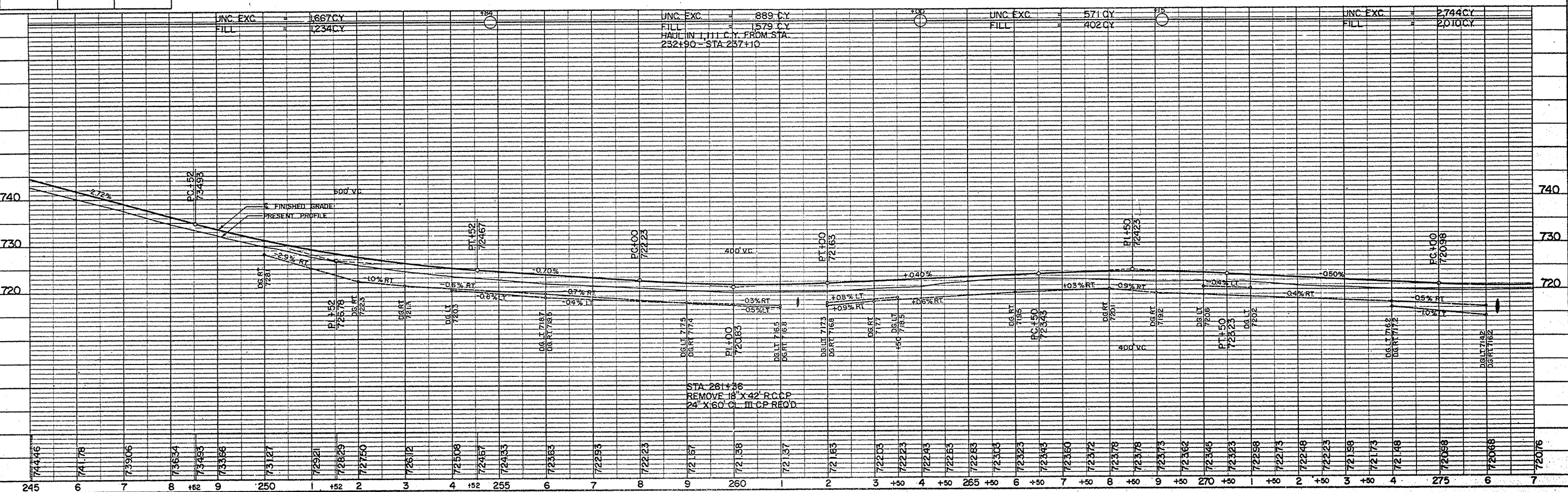
PROFILE
 CHECKED BY: [Signature]
 DATE: [Date]
 NO. 438

BENCH MARKS			
NO.	STATION	DESCRIPTION	ELEV.
41	249+20	SPIKE IN 12" MAPLE 52' LT.	731.39
42	258+85	6" B. ELDER 75' LT.	718.58
43	265+92	PT. MK. S.E. COR. BOTT. STEP 45' LT.	724.89
44	270+85	SPIKE IN 12" ASH 81' RT.	721.58
45	276+55	10" B. ELDER 75' RT.	717.71

B.P.R. DIVISION	PROJECT	SHEET NUMBER	TOTAL SHEETS
4	S 0377 (2)	10	37

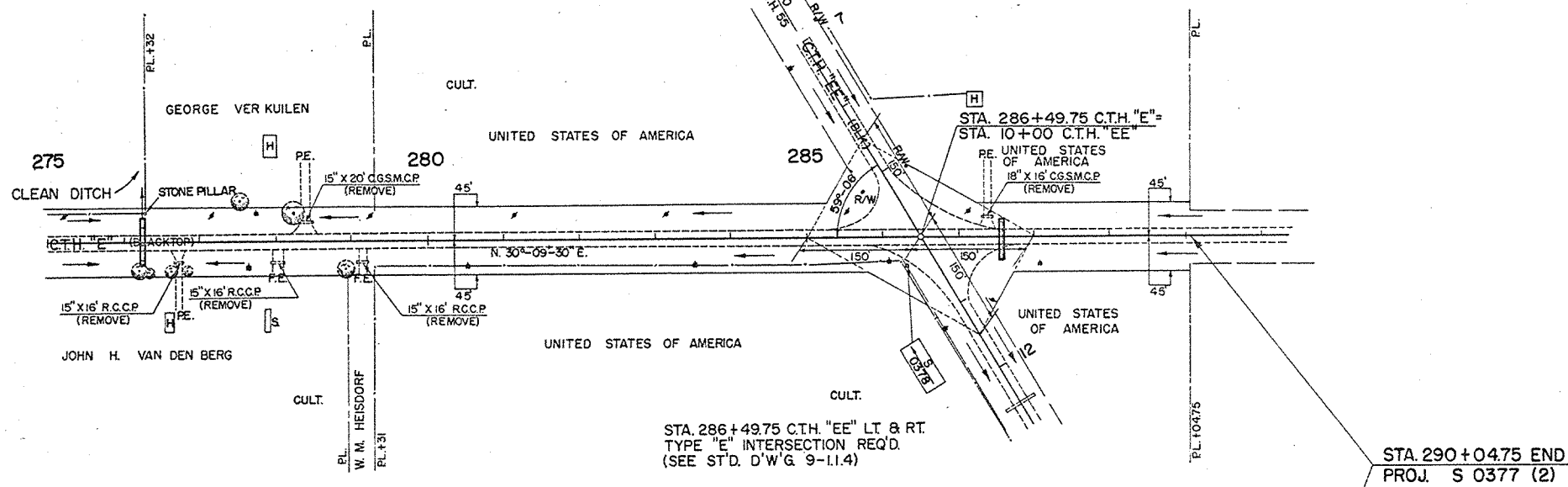


NET LENGTH OF CENTERLINE		
STATION TO STATION	LIN. FT.	
245+00	275+00	2,998.50



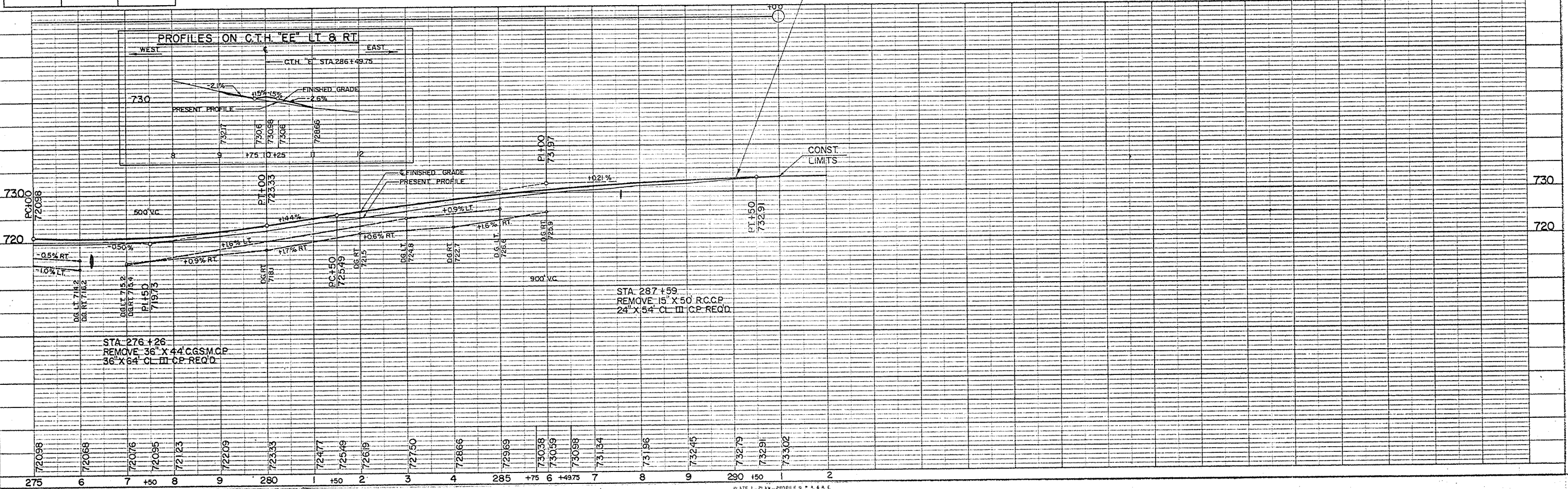
BENCH MARKS

NO.	STATION	DESCRIPTION	ELEV.
45	276+55	SPIKE IN 10" B.E. 75' RT.	717.71
46	287+18	" " 36" " 140' LT.	736.05



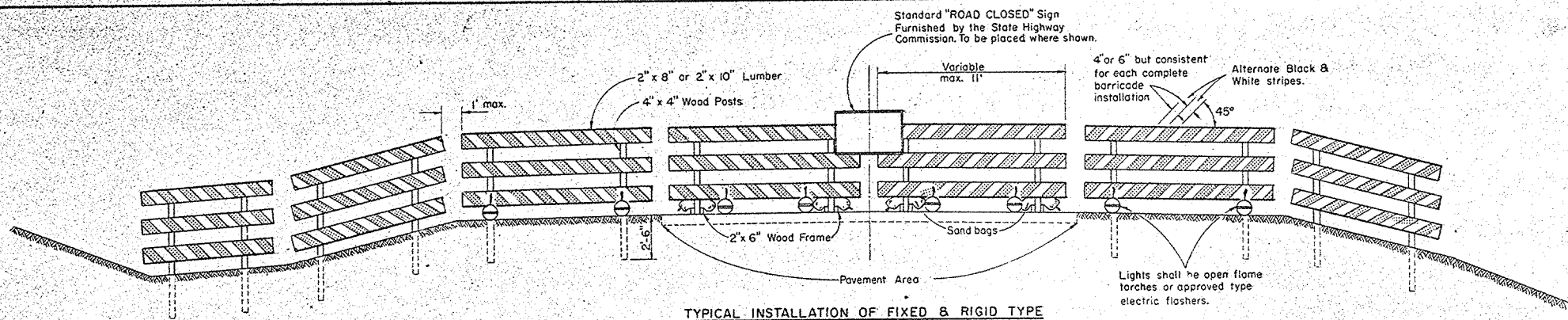
NET LENGTH OF CENTERLINE

STATION TO STATION	LIN. FT.	
275+00	290+04.75	1504.75

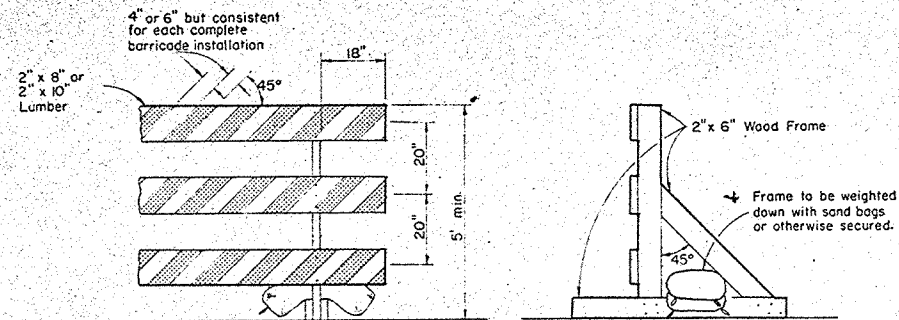


PLAN
 DATE: 5-68
 DRAWN BY: G. N. V.
 CHECKED BY: D. W. L.
 NO. 438

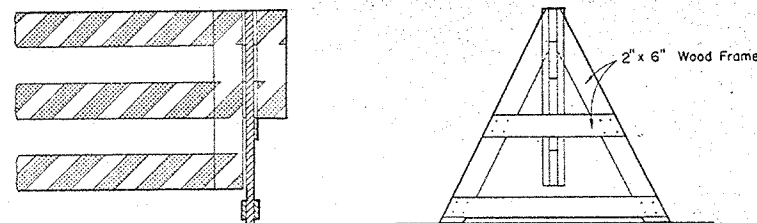
PROFILE
 DATE: 5-68
 DRAWN BY: G. N. V.
 CHECKED BY: D. W. L.
 NO. 439



TYPICAL INSTALLATION OF FIXED & RIGID TYPE

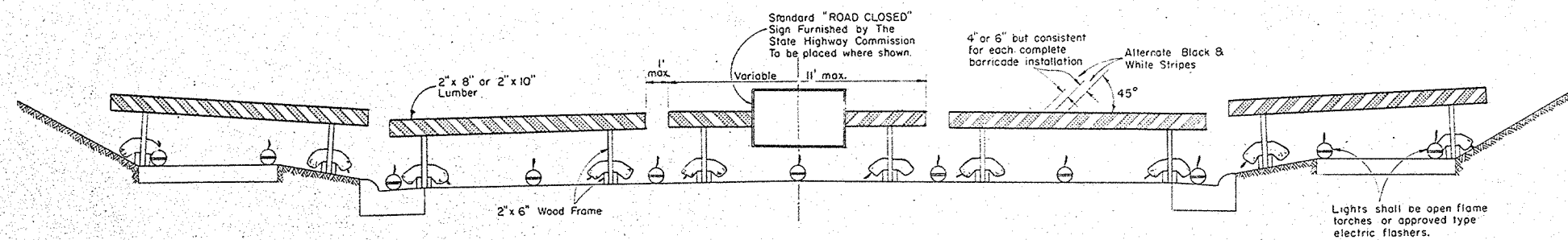


ALTERNATE TYPE INSTALLATION (RIGID)

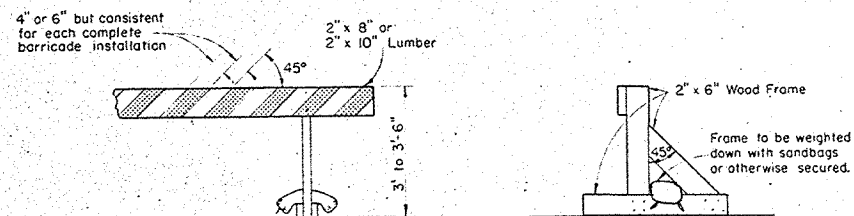


ALTERNATE TYPE INSTALLATION (DEMOUNTABLE)

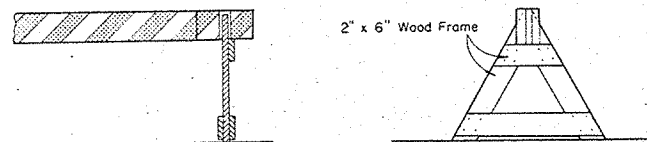
CLASS I BARRICADE



TYPICAL INSTALLATION OF RIGID TYPE



ALTERNATE TYPE INSTALLATION (RIGID)



ALTERNATE TYPE INSTALLATION (DEMOUNTABLE)

CLASS II BARRICADE

13-37

GENERAL NOTES:

The Contractor shall construct, place and maintain barricades as shown on this drawing and as required by the Standard Specifications for the duration of the project at all points of highway closure. Barricades shall be painted as shown hereon and structurally maintained for maximum visibility at all times, for the duration of the respective project.

CLASS I BARRICADE

Shall be used at points of closure where road is closed to traffic. Gates or movable sections of barricade shall be provided when necessary, for access of equipment or other authorized vehicles only.

CLASS II BARRICADE

May be used only where the hazard to traffic is relatively small, and for the more or less continuous delimiting of a restricted roadway, or for temporary daytime use.

LUMBER & FABRICATION

Lumber shall be of a grade structurally sound and sufficiently rigid to satisfactorily support and maintain the purpose and intent of a barricade facility. The fabrication of the barricade shall be in accord with good pertinent wood-working practices.

PAINTING

Barricades shall be painted as shown hereon in alternate black and white stripes. Black stripes shall be painted with weather resistant and durable black paint. White stripes shall be painted a prime coat of good grade wood primer, followed by two coats of white 'Coclit Reflective Liquid' (Minnesota Mining Co.) or equivalent, or reflective sheeting wide angle, flat top 'Scotchite' brand material (Minnesota Mining Co.) or equivalent.

DIRECTION OF DIAGONAL STRIPES

Where a barricade extends entirely across the roadway and no vehicle access provision, the stripes shall slope downward toward the highway centerline.

Where vehicle access is permitted, the stripes shall slope downward in the direction toward which vehicles must turn in detouring.

Where both right and left turns are provided for, the stripes shall slope downward in both directions from the center.

MEASUREMENT & PAYMENT

All barricades, unless otherwise provided for in the plans and/or special provisions shall be furnished, placed, and maintained as noted above, and no additional compensation will be allowed but shall be construed to be included in the price bid for other items.

NOTE:

Lighting devices for barricades shall conform to the requirements of the Standard Specifications.

NOTE:

All lumber or timber dimensions shown hereon are nominal.

CONSTRUCTION BARRICADE

STATE HIGHWAY COMMISSION OF WISCONSIN

RECOMMENDED FOR APPROVAL

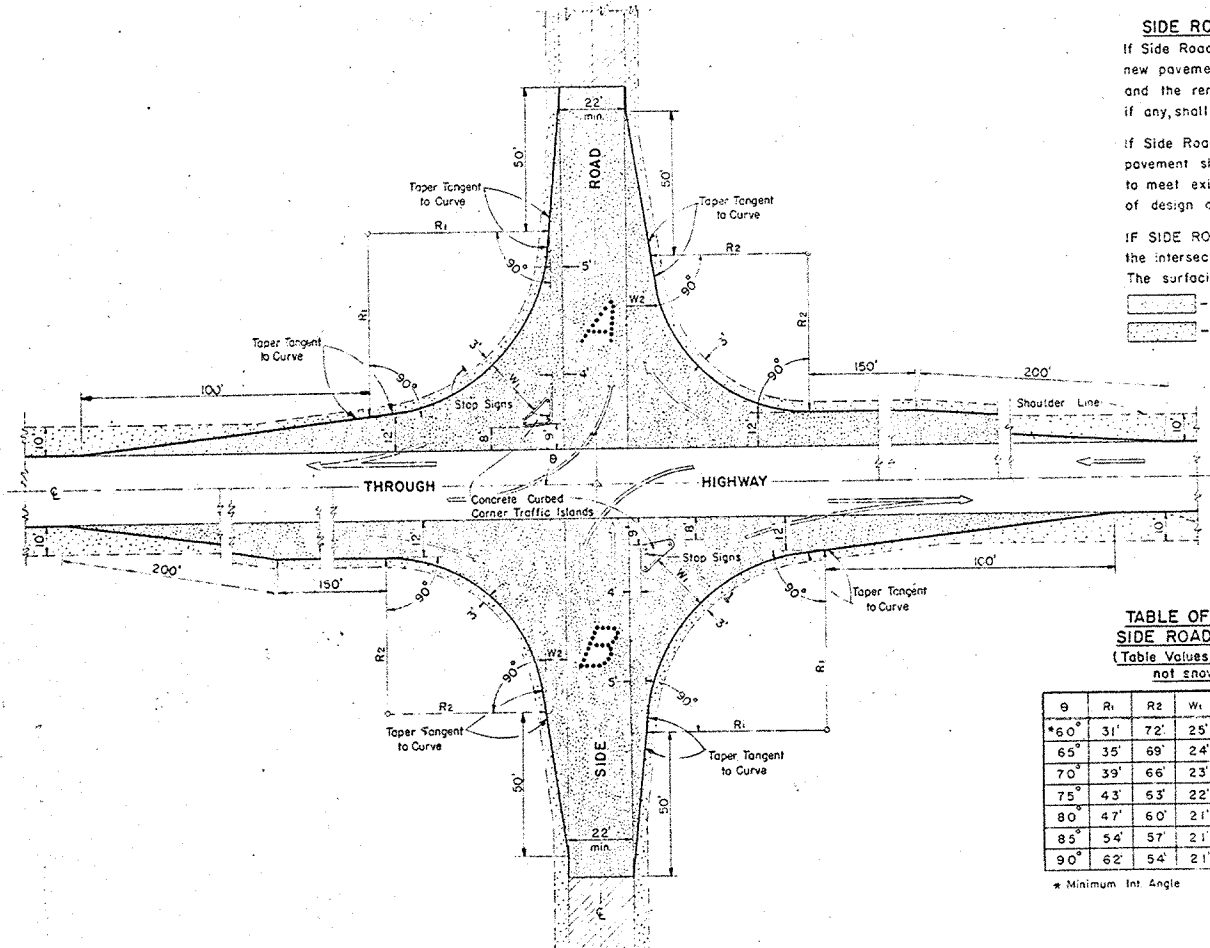
DATE 2-5-63 J. P. Pitt ENGINEER OF DESIGN

APPROVED

DATE 2/16/63 P. C. Rustigan STATE HIGHWAY ENGINEER

PLATE NO. 7-4.1.4

15-37



SIDE ROAD SURFACING NOTE
 If Side Road is not presently surfaced, new pavement shall be placed as shown, and the remainder to construction limits, if any, shall be gravel or crushed stone surfaced.
 If Side Road is presently paved, new pavement shall be placed only as necessary to meet existing pavement, and to limits of design as shown.
 IF SIDE ROAD IS THE CONSTRUCTION PROJECT, the intersection geometrics remain as shown. The surfacing shall be same as for the project.
 - Pavement
 - Gravel or Crushed Stone

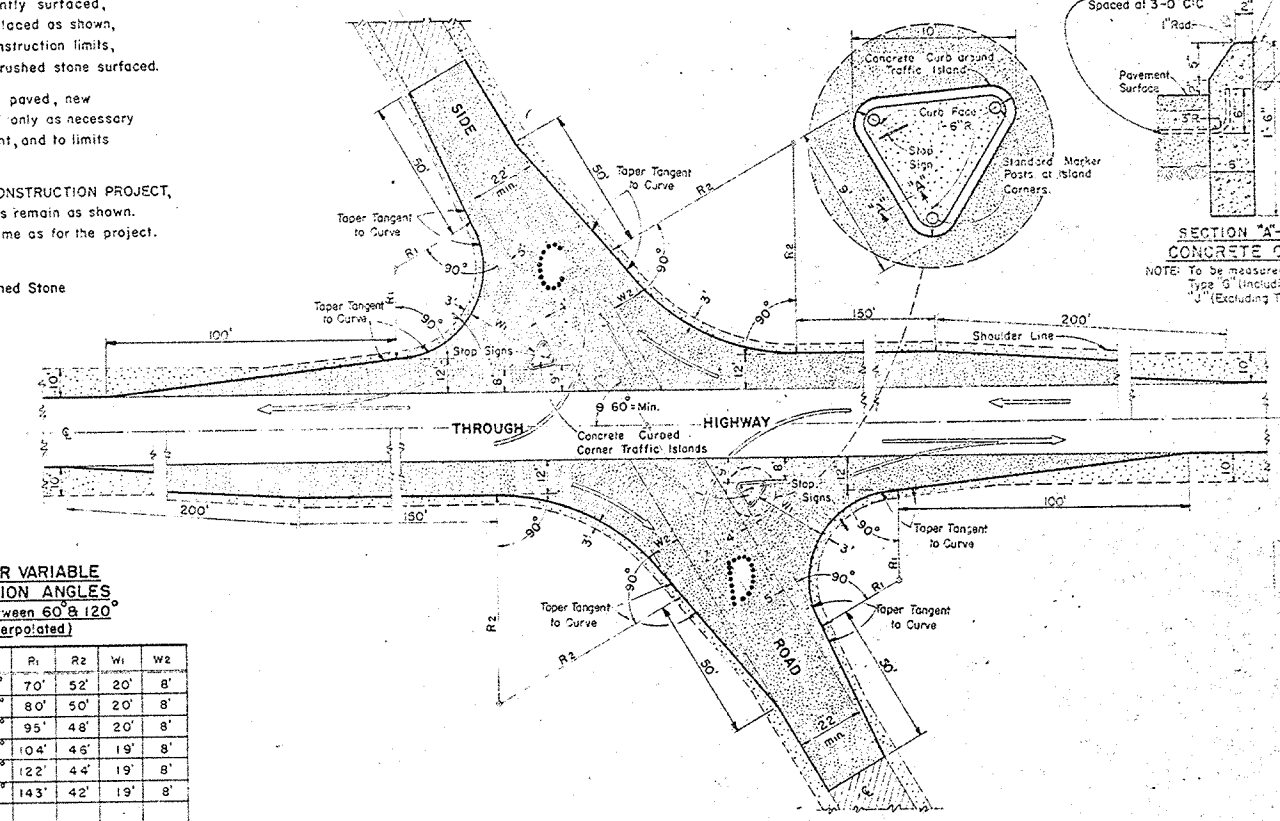


TABLE OF VALUES FOR VARIABLE SIDE ROAD INTERSECTION ANGLES
 (Table Values for Angles between 60° & 120° not shown shall be interpolated)

θ	R ₁	R ₂	W ₁	W ₂	θ	R ₁	R ₂	W ₁	W ₂
60°	31'	72'	25'	10'	95°	70'	52'	20'	8'
65°	35'	69'	24'	9'	100°	80'	50'	20'	8'
70°	39'	66'	23'	8'	105°	95'	48'	20'	8'
75°	43'	63'	22'	8'	110°	104'	46'	19'	8'
80°	47'	60'	21'	8'	115°	122'	44'	19'	8'
85°	54'	57'	21'	8'	120°	143'	42'	19'	8'
90°	62'	54'	21'	8'					

* Minimum Int. Angle † Maximum Int. Angle

MAJOR SIDE ROAD INTERSECTION DESIGN DETAILS

To be used only when current ADT on Through Highway is 1500 or over, and on Side Road is Over 200

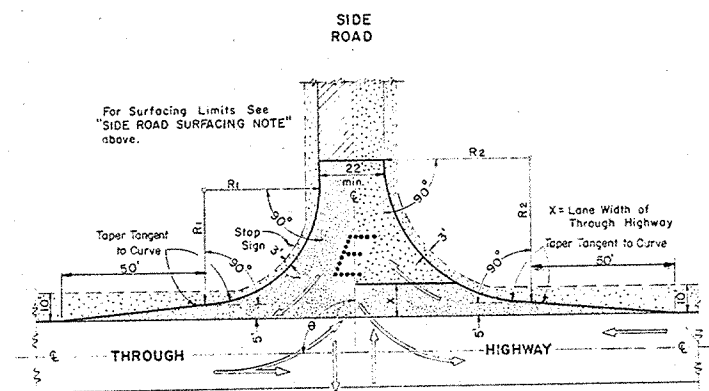


TABLE OF VALUES FOR VARIABLE SIDE ROAD INTERSECTION ANGLES
 (Table Values for Angles between 60° & 120° not shown shall be interpolated)

θ	R ₁	R ₂	θ	R ₁	R ₂
60°	40'	50'	95°	40'	49'
65°	40'	50'	100°	50'	48'
70°	40'	50'	105°	55'	47'
75°	40'	50'	110°	60'	46'
80°	40'	50'	115°	65'	45'
85°	40'	50'	120°	70'	44'
90°	40'	50'			

* Minimum Int. Angle ** Maximum Int. Angle

MINOR SIDE ROAD INTERSECTION DESIGN DETAILS

To be used when current ADT on Through Highway is Less than 1500 or on Side Road is Less than 200

GENERAL NOTES
 Designs "A", "B", "C", "D" or "E" may be used interchangeably in combination or separately for any one complete intersection depending upon Traffic Volume, intersection angle and Surfacing of each approach roadway.

Details on this drawing are for Minimum Design Only, and not applicable to Special Conditions, as shown elsewhere on the plans.

DESIGN & LAYOUT DETAILS FOR SIDE ROAD AT GRADE INTERSECTIONS (RURAL IN CHARACTER)

STATE HIGHWAY COMMISSION OF WISCONSIN

RECOMMENDED FOR APPROVAL:

DATE: 2-5-63 *J.P. Pitt*

APPROVED: *E.L. Rosthorne*

DATE: 2/6/63

PLATE NO. 2-11