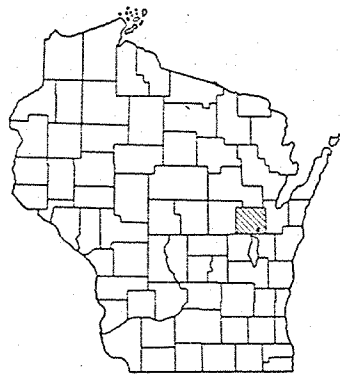


INDEX OF SHEETS

SHEET NO. 1 TITLE
 SHEET NO. 2-24 TYPICAL CROSS SECTIONS & LAYOUT DRAWINGS
 SHEET NO. 3 ESTIMATE OF QUANTITIES
 SHEET NO. 3A MISCELLANEOUS QUANTITIES
 SHEET NO. — RIGHT OF WAY PLAT
 SHEET NO. 4-12 PLAN AND PROFILE STA. 10+24 TO STA. 62+12
 SHEET NO. 13-24 STANDARD DETAILS
 SHEET NO. — DRAINAGE STRUCTURES
 SHEET NO. 25-33 CROSS SECTIONS

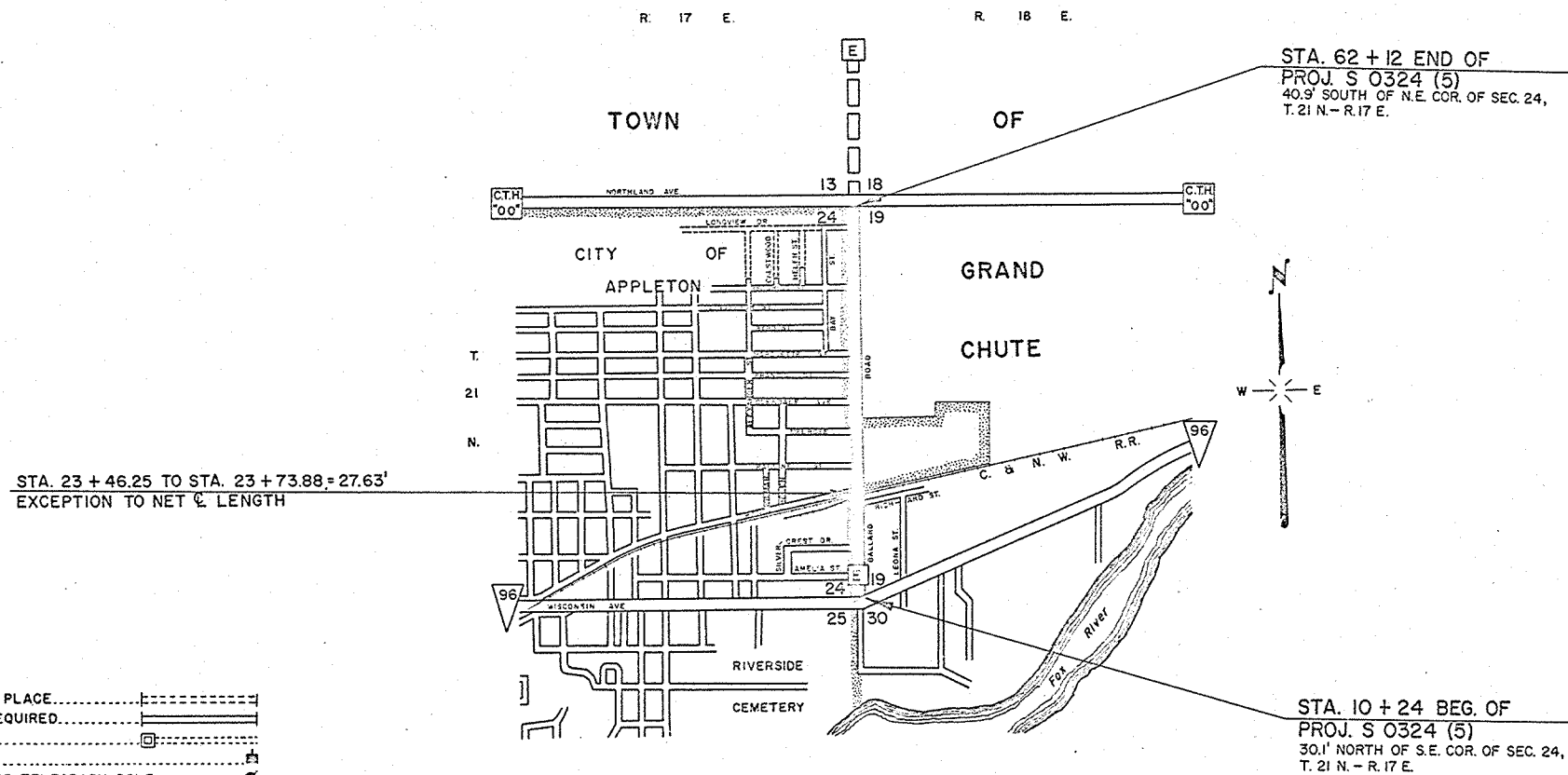


STATE OF WISCONSIN
 STATE HIGHWAY COMMISSION OF WISCONSIN

PLAN AND PROFILE OF PROPOSED
 S.T.H. 96 — U.S.H. 41 ROAD
 (BALLARD ROAD)
 C.T.H. "E"
 OUTAGAMIE COUNTY
 PROJECT S 0324 (3)

COUNTY AND HIGHWAY	ROUTE AND SECTION	CLASS AND AGREEMENT		S.P.R. REGION DIVISION	SHEET NUMBER	TOTAL SHEETS
		STATE	FEDERAL			
44.6	324.0		13.5	4 WIS.	1	33

PLAN 1 IN. = 20 FT.
 PROFILE HOR. 1 IN. = 20 FT. VERT. 1 IN. = 2 FT.
 CROSS SECTIONS HOR. 1 IN. = 5 FT. VERT. 1 IN. = 5 FT.



County Project #115
S 0324(5) -
E-9

CONVENTIONAL SIGNS

STATE LINE.....	CULVERTS IN PLACE.....
COUNTY LINE.....	CULVERTS REQUIRED.....
TOWNSHIP OR RANGE LINE.....	DROP INLET.....
SECTION LINE.....	POWER POLE.....
NEW RIGHT OF WAY LINE.....	TELEPHONE OR TELEGRAPH POLE.....
PRESENT RIGHT OF WAY LINE.....	RIGHT OF WAY MARKERS.....
WIRE FENCE { WOVEN.....	REFERENCE STAKE FOR HUBS ONLY.....
{ BARBED.....	MARSH.....
LOT LINE.....	HEDGE.....
CORPORATE OR CITY LIMITS.....	TREES.....
PROPERTY LINE.....	GROUND ELEVATION..... DATUM LINE 7516
TRAVELED WAY OR P.E.....	GRADE ELEVATION..... DATUM LINE 7516
RAILROADS.....	
BASE OR SURVEY LINE.....	

LAYOUT
 SCALE 1" = 1000'
 IN FEET

NET LENGTH OF CENTERLINE = .786 MI. (RURAL)
 " " " = .191 MI. (URBAN)
 TOTAL = .977 MI.

STATE HIGHWAY COMMISSION OF WISCONSIN
 MADISON, WIS.

SUPERVISOR..... NOTE BOOK 332-700
 DISTRICT CHECKER..... W. O. CHECKER, DEC.
 DISTRICT CHECKER..... L. J. L. CORSEY

CORRECT:
 DATE 2-27-63 *[Signature]*
 DISTRICT ENGINEER

RECOMMENDED FOR APPROVAL:
 DATE 3-29-63 *[Signature]*
 ENGINEER OF DESIGN

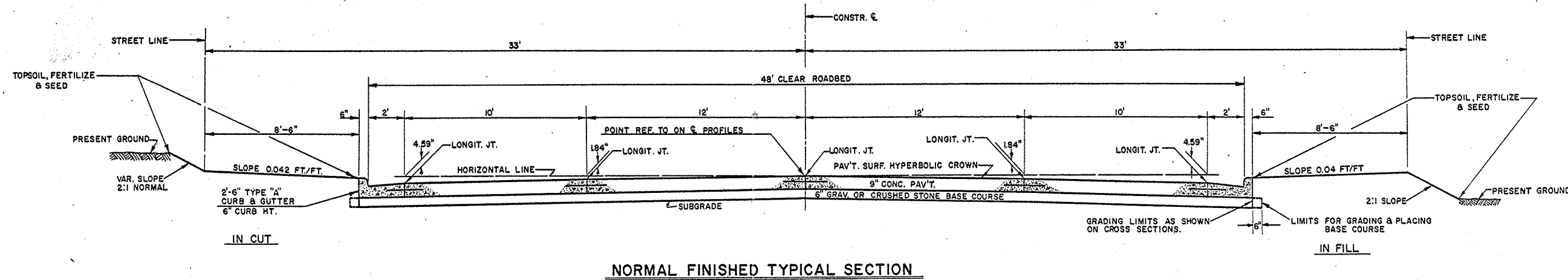
APPROVED:
 DATE 3/29/63 *[Signature]*
 STATE HIGHWAY ENGINEER

DEPARTMENT OF COMMERCE
 BUREAU OF PUBLIC ROADS

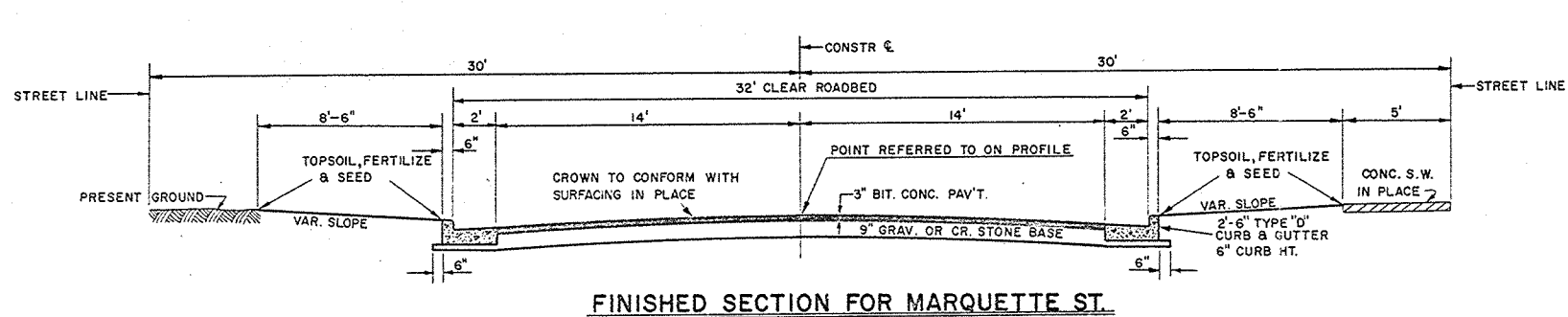
APPROVED: _____ DATE _____
 DIVISION ENGINEER

E-9

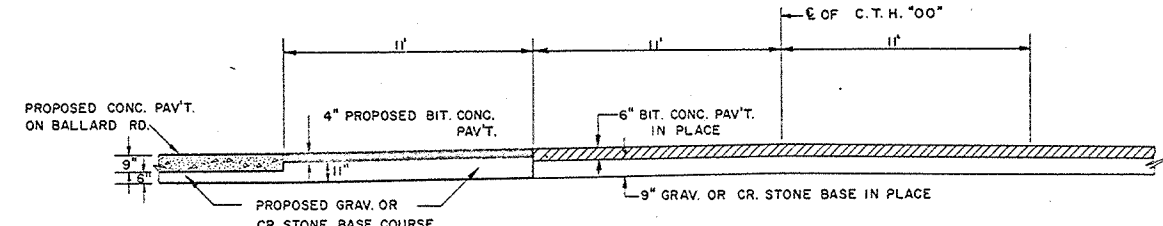
S 0324(5)



NORMAL FINISHED TYPICAL SECTION



FINISHED SECTION FOR MARQUETTE ST.



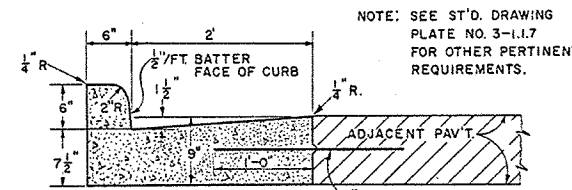
SECTION FOR PROPOSED CONNECTION AT BALLARD RD. & C.T.H. "00"

GENERAL NOTES

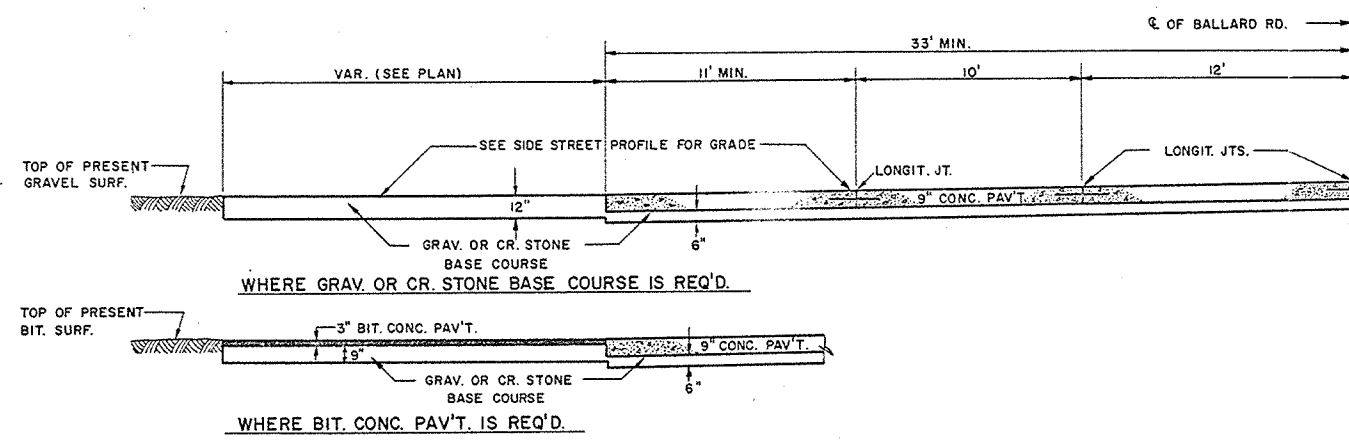
DETAILS OF CONSTRUCTION NOT SHOWN SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, STANDARD DRAWINGS AND THE APPLICABLE SPECIAL PROVISIONS.
 CONC. PAV'T. REINF. REQ'D. IN TRAVEL AND PARKING LANES ON BALLARD ROAD. NONE REQ'D. IN PAV'T. FOR SIDE STREET APPROACHES.
 WHEN THE QUANTITY OF THE ITEMS OF SUBBASE, BASE OR SURFACE COURSE IS MEASURED FOR PAYMENT BY THE TON OR CUBIC YARD, THE DEPTH OR THICKNESS OF THE COURSE SHOWN ON THE PLANS IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL DIRECTED BY THE ENGINEER.
 THIS PROJECT IS DESIGNED FOR PARALLEL PARKING. CONTRACTION JOINTS FOR URBAN TYPE PAVEMENTS MAXIMUM SPACING 50 FT. PLUS OR MINUS (40 FT. MINIMUM.)
 SALVAGED TOPSOIL TO BE PLACED ON ALL CUT AND FILL SLOPES TO APPROXIMATELY 3" DEPTH AT TIME OF PLACING.

STANDARD DRAWINGS

CONC. PAV'T. REINF.	2-1.1.11
BRIDGE & RR. APPR. DETAILS	2-2.1.10
CONC. C. & G. AND SURFACE DRAINS	3-1.1.7
LONGIT. JOINTS FOR CONC. PAV'T.	4-4.4.6
TRANSVERSE JOINTS FOR CONC. PAV'T.	4-4.5.9
CATCH BASINS & INLET COVERS	5-3.4.6
INLETS	5-3.5.1
MANHOLES	5-3.7.2
MANHOLES COVERS	5-3.8.2
CONSTRUCTION BARRICADE	7-4.1.4
LANDMARK REFERENCE MONUMENTS	12-1.1.2
METAL & FIBER CONDUIT	8-5.1.5



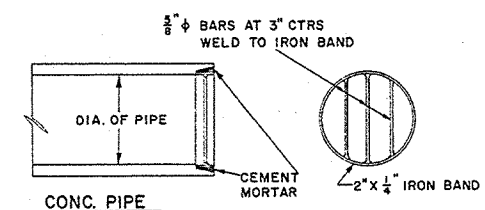
CONC. CURB & GUTTER (BARRIER TYPE)



SECTION ALONG C OF SIDE STREET APPROACHES

X - FEET	0	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25
Y - INCH	0"	1/32"	1/16"	3/32"	1/4"	5/32"	3/16"	3/8"	1/2"	5/8"	3/4"	7/8"	1 1/8"	1 1/4"	1 3/8"	1 1/2"	1 5/8"	1 3/4"	1 7/8"	2"	2 1/8"	2 1/4"	2 3/8"	2 1/2"	2 5/8"	2 3/4"
Y - INCH	0.00"	0.03"	0.06"	0.16"	0.25"	0.41"	0.56"	0.75"	0.94"	1.12"	1.37"	1.59"	1.84"	2.09"	2.34"	2.62"	2.87"	3.16"	3.44"	3.72"	4.00"	4.28"	4.59"	4.87"	5.16"	5.47"

COORDINATES FOR HYPERBOLIC CROWN



END OF PIPE GRATE
 FOR STATIONS SEE STORM SEWER LISTING ON MISC. QUANTITIES SHEET

TYPICAL CROSS SECTIONS FOR

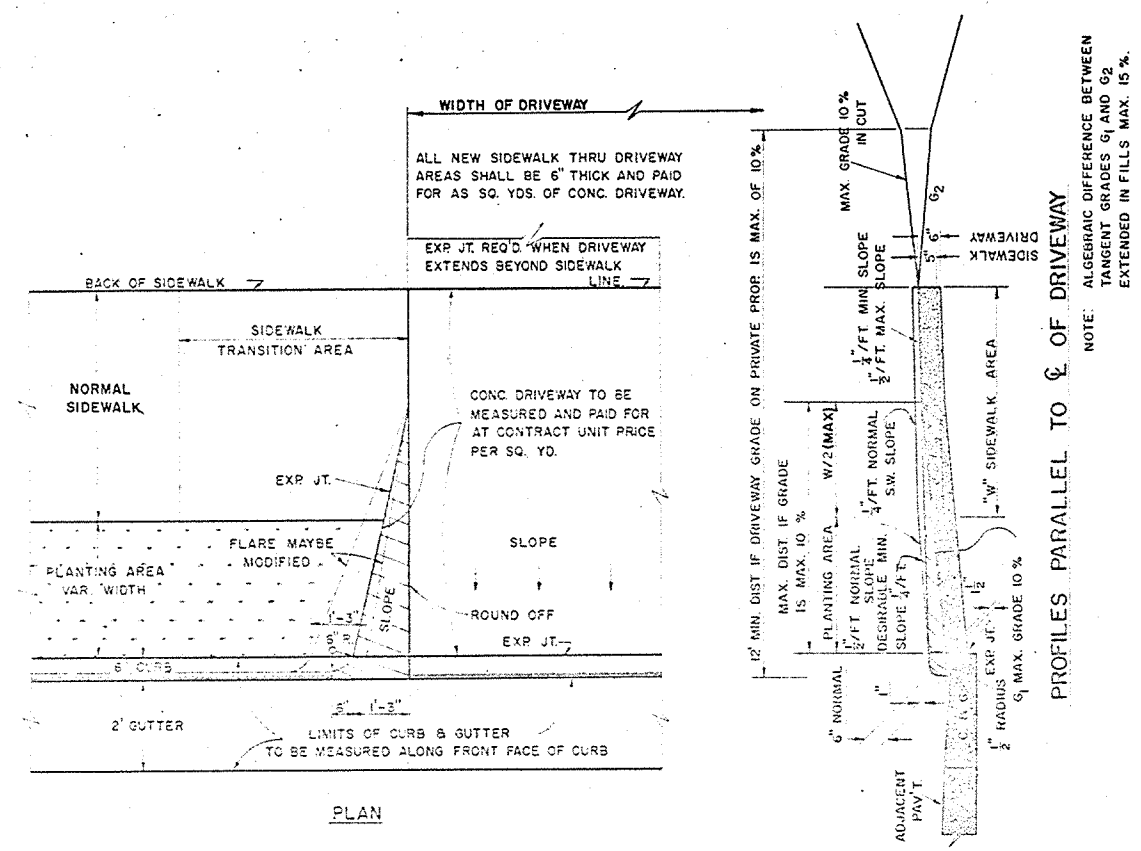
44 FT. CONC. PAV'T. - 49 FT. ROADBED

28 FT. BIT. PAV'T. - 33 FT. ROADBED

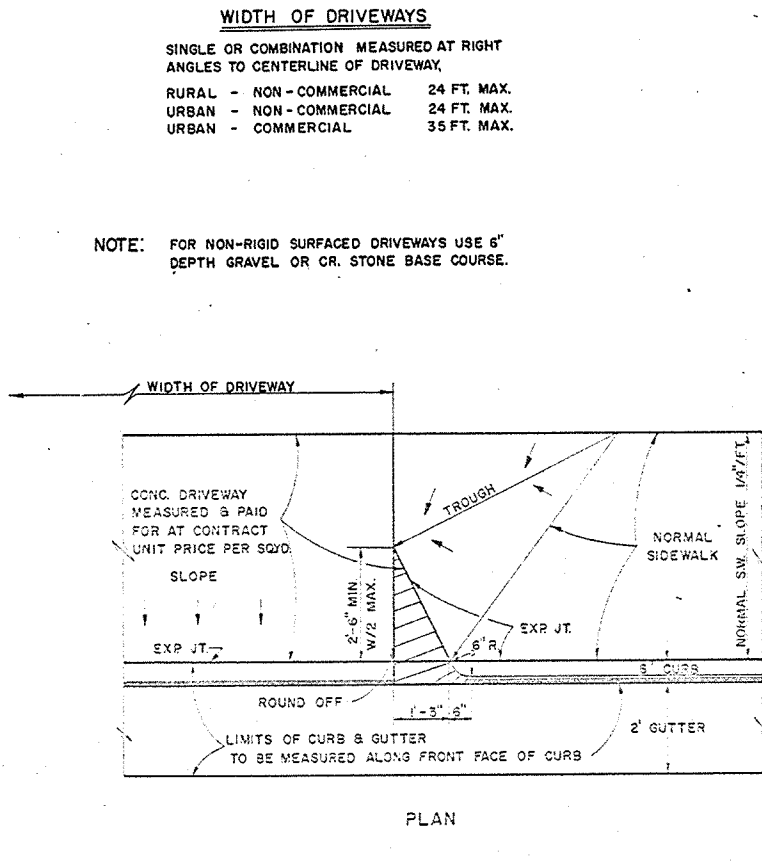
AND

MISC. CONSTRUCTION

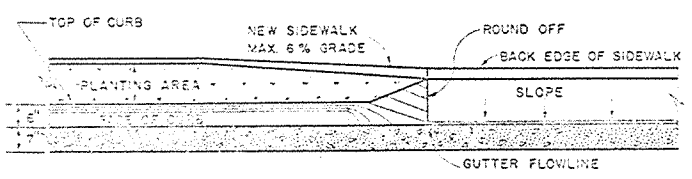
SCALE VARIABLE



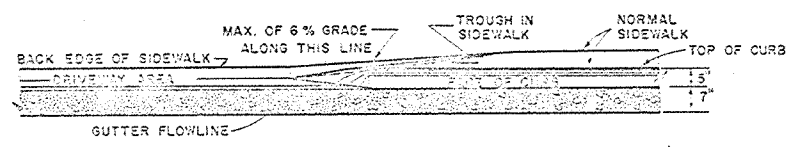
WHEN SIDEWALK IS SEPARATED FROM CURB BY PLANTING AREA



WHEN SIDEWALK IS IMMEDIATELY ADJACENT TO CURB



PROFILES PARALLEL TO CENTERLINE OF ROADWAY



PROFILES PARALLEL TO CENTERLINE OF ROADWAY

PROFILES PARALLEL TO \bar{C} OF DRIVEWAY

PROFILES PARALLEL TO \bar{C} OF DRIVEWAY

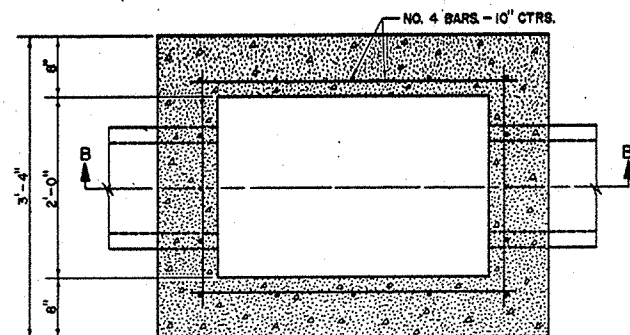
GENERAL NOTES

FOR ALL OTHER CONSTRUCTION DETAILS AND REQUIREMENTS FOR THE WORK DETAILED ON THIS DRAWING SEE SECTIONS 409, 601 & 602 OF THE STANDARD SPECIFICATIONS. CONCRETE USED IN THE WORK OF "CONCRETE DRIVEWAYS" SHALL BE GRADE "AA" CONCRETE USED IN THE WORK OF "CONCRETE CURB & GUTTER" AND "CONCRETE SIDEWALKS" SHALL BE GRADE "AA" IN CONFORMITY WITH THE STANDARD SPECIFICATIONS.

DRIVEWAY DETAILS

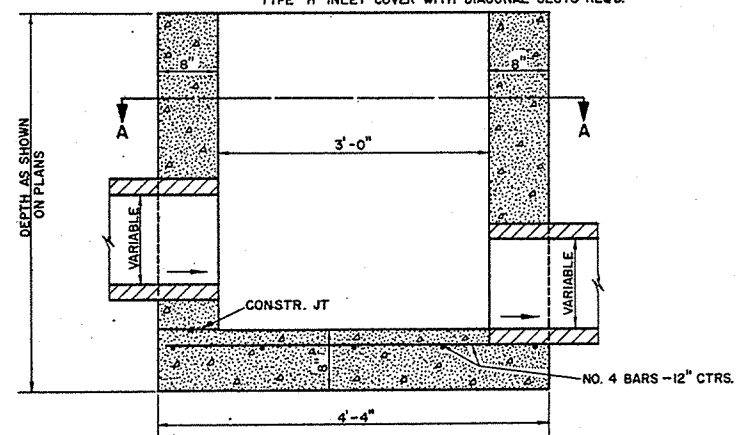
CONSTRUCTION DETAILS
FOR
DRIVEWAYS
VARIABLE SCALE

PROJECT	SHEET NUMBER	TOTAL SHEETS
S 0324 (5)	22	33



SECTION A-A

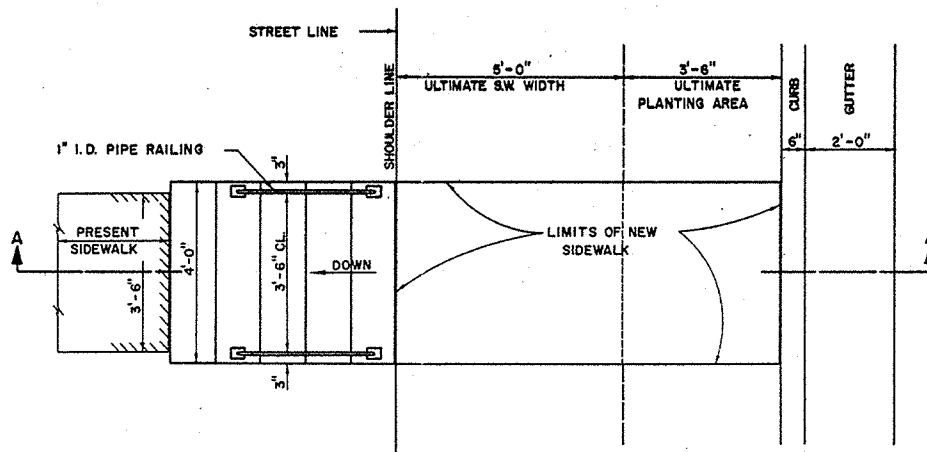
NOTE: SEE ST'D. DRAWINGS NO. 5-3.4.6 & 5-3.5.1 FOR OTHER PERTINENT REQUIREMENTS. TYPE "H" INLET COVER WITH DIAGONAL SLOTS REQ'D.



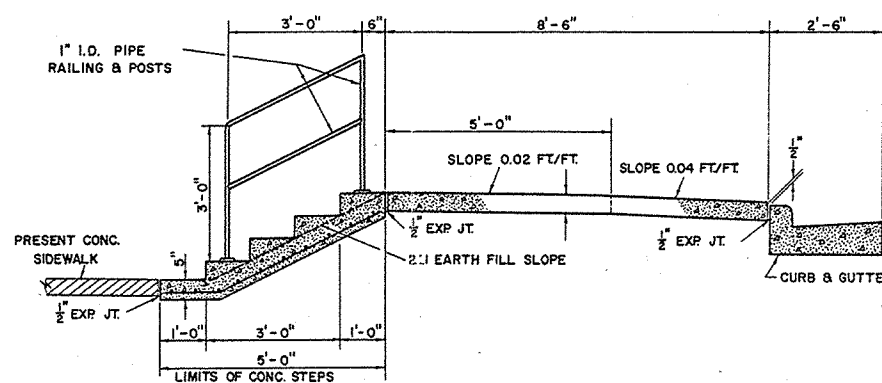
SECTION B-B

TYPE 3 INLET

REINF. CONC.



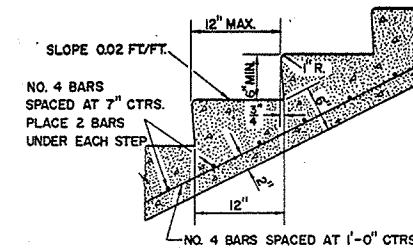
PLAN



SECTION A-A

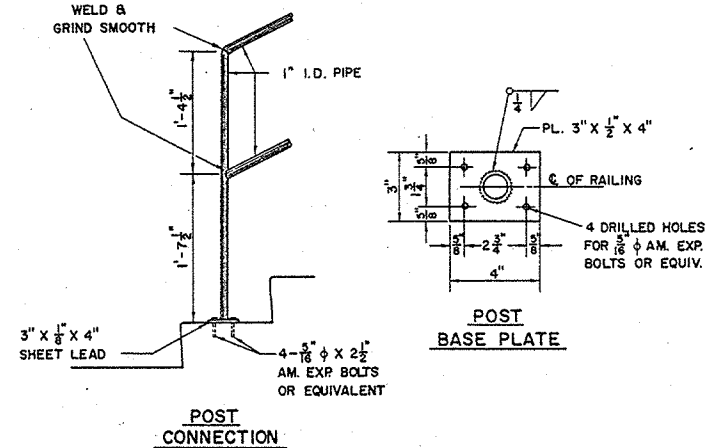
DETAILS FOR CONC. STEPS & PIPE RAILING WITH PIPE POSTS

STA. 25+00 LT.

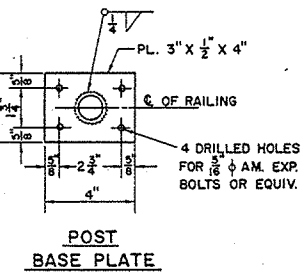


STEP DETAIL

GENERAL NOTES
 THE EXACT WIDTH OF TREAD AND RISE OF STEPS TO BE DETERMINED BY THE ENGINEER IN THE FIELD.
 STEEL REINF. NOT REQ'D. ON STEPS WITH 2 RISERS OR LESS.
 NO PIPE RAILING REQ'D. ON STEPS WITH 3 RISERS OR LESS.
 THE EXACT LOCATIONS & NUMBER OF STEPS TO BE DETERMINED BY THE ENGINEER IN THE FIELD.



POST CONNECTION

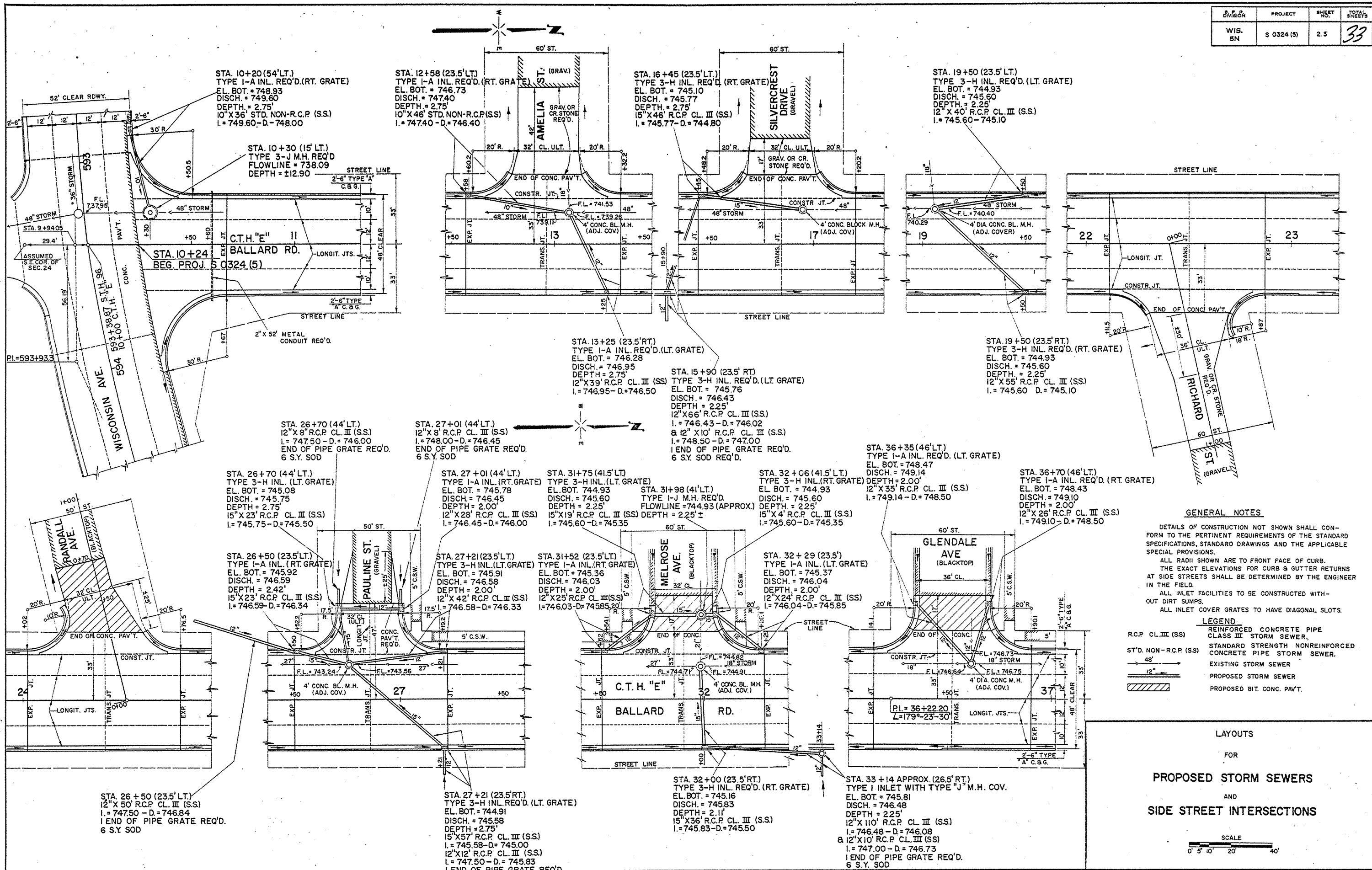


POST BASE PLATE

DETAILS FOR
 TYPE 3 INLET
 AND
 CONC. STEPS WITH PIPE RAILING

SCALE - VARIABLE

S. P. R. DIVISION	PROJECT	SHEET NO.	TOTAL SHEETS
WIS. SN	S 0324 (5)	23	33



GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, STANDARD DRAWINGS AND THE APPLICABLE SPECIAL PROVISIONS.

ALL RADII SHOWN ARE TO FRONT FACE OF CURB. THE EXACT ELEVATIONS FOR CURB & GUTTER RETURNS AT SIDE STREETS SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.

ALL INLET FACILITIES TO BE CONSTRUCTED WITHOUT DIRT SUMPS.

ALL INLET COVER GRATES TO HAVE DIAGONAL SLOTS.

LEGEND

R.C.P. CL. III (SS) REINFORCED CONCRETE PIPE CLASS III STORM SEWER.

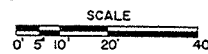
STD. NON-R.C.P. (SS) STANDARD STRENGTH NONREINFORCED CONCRETE PIPE STORM SEWER.

48" EXISTING STORM SEWER

12" PROPOSED STORM SEWER

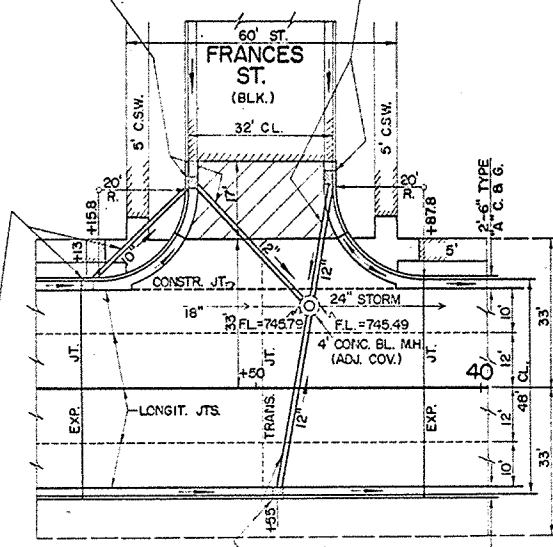
PROPOSED BIT. CONC. PAVT.

LAYOUTS FOR PROPOSED STORM SEWERS AND SIDE STREET INTERSECTIONS



S.P.D. DIVISION	PROJECT	SHEET NO.	TOTAL SHEETS
WIS. 5N	S 0324 (5)	2.4	33

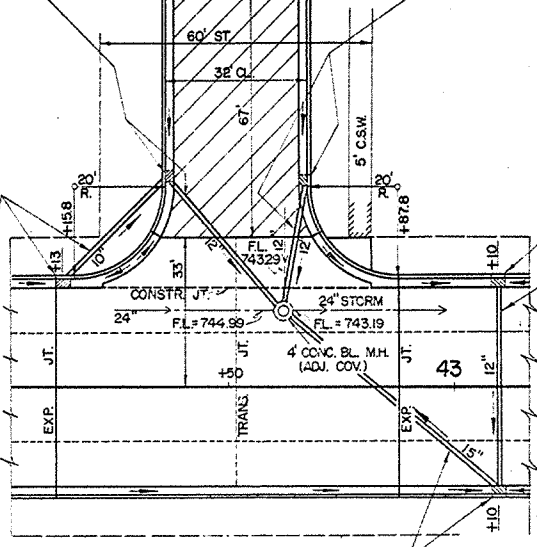
STA. 39+36 (46.5' LT.)
TYPE 3-H INL. REQ'D. (LT. GRATE)
EL. BOT. = 745.97
DISCH. = 746.64
DEPTH = 2.50'
12" X 34" R.C.P. CL. III (S.S.)
I = 746.64 - D = 746.30



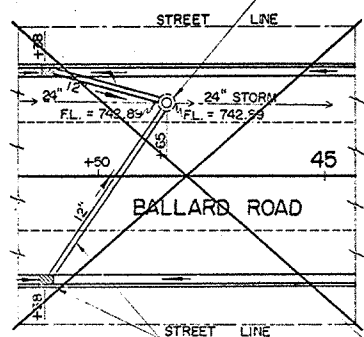
STA. 39+67 (46.5' LT.)
TYPE 3-H INL. REQ'D. (RT. GRATE)
EL. BOT. = 746.21
DISCH. = 746.88
DEPTH = 2.25'
12" X 24" R.C.P. CL. III (S.S.)
I = 746.88 - D = 746.50

STA. 42+37 (46.5' LT.)
TYPE 3-H INL. (LT. GRATE)
EL. BOT. = 745.19
DISCH. = 745.86
DEPTH = 2.50'
12" X 35" R.C.P. CL. III (S.S.)
I = 745.86 - D = 745.50

STA. 42+68 (46.5' LT.)
TYPE I-A INL. REQ'D. (RT. GRATE)
EL. BOT. = 745.64
DISCH. = 746.31
DEPTH = 2.00'
12" X 26" R.C.P. CL. III (S.S.)
I = 746.31 - D = 745.49

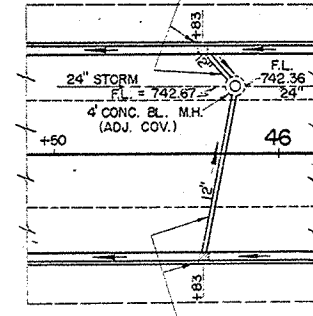


STA. 43+10 (23.5' LT.)
TYPE 3-H INL. REQ'D. (LT. GRATE)
EL. BOT. = 745.42
DISCH. = 746.09
DEPTH = 2.00'
12" X 44" R.C.P. CL. III (S.S.)
I = 746.09 - D = 745.84

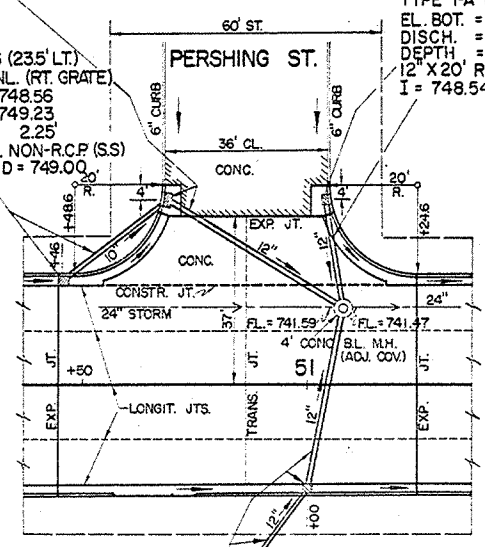


STA. 44+65 (7' LT.)
TYPE I-A INL. REQ'D.
EL. BOT. = 742.87
DEPTH = 4.16'

STA. 45+83 (23.5' LT.)
TYPE I-A INL. (LT. GRATE)
EL. BOT. = 746.15
DISCH. = 746.82
DEPTH = 2.75'
12" X 7" R.C.P. CL. III (S.S.)
I = 746.82 - D = 746.32



STA. 50+46 (23.5' LT.)
TYPE I-A INL. (RT. GRATE)
EL. BOT. = 748.56
DISCH. = 749.23
DEPTH = 2.25'
10" X 24" STD. NON-R.C.P. (S.S.)
I = 749.23 - D = 749.00



STA. 51+04 (40' LT.)
TYPE I-A INL. (RT. GRATE)
EL. BOT. = 747.87
DISCH. = 748.54
DEPTH = 2.75'
12" X 20" R.C.P. CL. III (S.S.)
I = 748.54 - D = 747.50

STA. 39+13 (23.5' LT.)
TYPE I-A INL. REQ'D. (RT. GRATE)
EL. BOT. = 746.67
DISCH. = 747.34
DEPTH = 2.00'
10" X 28" STD. NON-R.C.P. (S.S.)
I = 747.34 - D = 747.00

STA. 39+55 (23.5' RT.)
TYPE I-A INL. REQ'D. (LT. GRATE)
EL. BOT. = 746.46
DISCH. = 747.13
DEPTH = 2.00'
12" X 38" R.C.P. CL. III (S.S.)
I = 747.13 - D = 746.63

STA. 42+13 (23.5' LT.)
TYPE 3-H INL. (RT. GRATE)
EL. BOT. = 745.71
DISCH. = 746.38
DEPTH = 2.00'
10" X 28" STD. NON-R.C.P. (S.S.)
I = 746.38 - D = 746.00

STA. 43+10 (23.5' RT.)
TYPE 3-H INL. REQ'D. (LT. GRATE)
EL. BOT. = 744.92
DISCH. = 745.59
DEPTH = 2.50'
15" X 58" R.C.P. CL. III (S.S.)
I = 745.59 - D = 745.00

STA. 44+38 (23.5' RT.)
TYPE 3-H INL. REQ'D. (LT. GRATE)
EL. BOT. = 745.04
DISCH. = 745.71
DEPTH = 2.00'
12" X 44" R.C.P. (S.S.)
I = 745.71 - D = 744.51

STA. 45+83 (23.5' RT.)
TYPE I-A INL. REQ'D. (RT. GRATE)
EL. BOT. = 746.15
DISCH. = 746.82
DEPTH = 2.75'
12" X 35" R.C.P. CL. III (S.S.)
I = 746.82 - D = 745.82

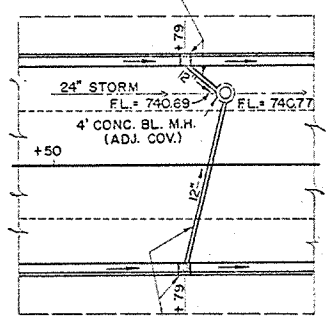
STA. 51+00 (23.5' RT.)
TYPE I-A INL. REQ'D. (LT. GRATE)
EL. BOT. = 747.90
DISCH. = 748.57
DEPTH = 2.75'
12" X 37" R.C.P. CL. III (S.S.)
I = 748.57 - D = 747.50
12" X 12" R.C.P. CL. III (S.S.)
I = 750.50 - D = 748.82
END OF PIPE GRATE REQ'D.
6 S.S. SOD

STA. 59+13 (23.5' LT.)
TYPE I-A INL. REQ'D. (RT. GRATE)
EL. BOT. = 742.94
DISCH. = 743.61
DEPTH = 2.75'
12" X 17" R.C.P. CL. III (S.S.)
I = 743.61 - D = 743.00

NOTE
ELEV'S SHOWN ARE APPROXIMATE.
CUT HOLES IN 54" R.C. PIPE FOR CONNECTIONS.

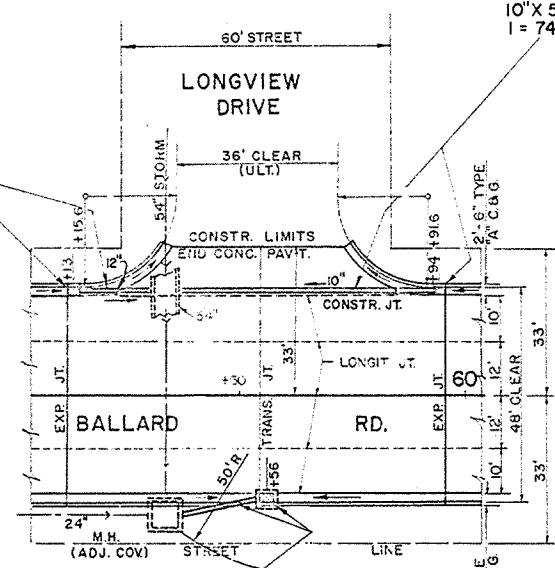
STA. 59+94 (23.5' LT.)
TYPE I-A INL. REQ'D. (LT. GRATE)
EL. BOT. = 742.89
DISCH. = 743.56
DEPTH = 2.75'
10" X 57" STD. NON-R.C.P. (S.S.)
I = 743.56 - D = 743.00

STA. 54+79 (23.5' LT.)
TYPE I-A INL. REQ'D. (RT. GRATE)
EL. BOT. = 746.76
DISCH. = 747.43
DEPTH = 2.75'
12" X 8" R.C.P. CL. III (S.S.)
I = 747.43 - D = 747.00



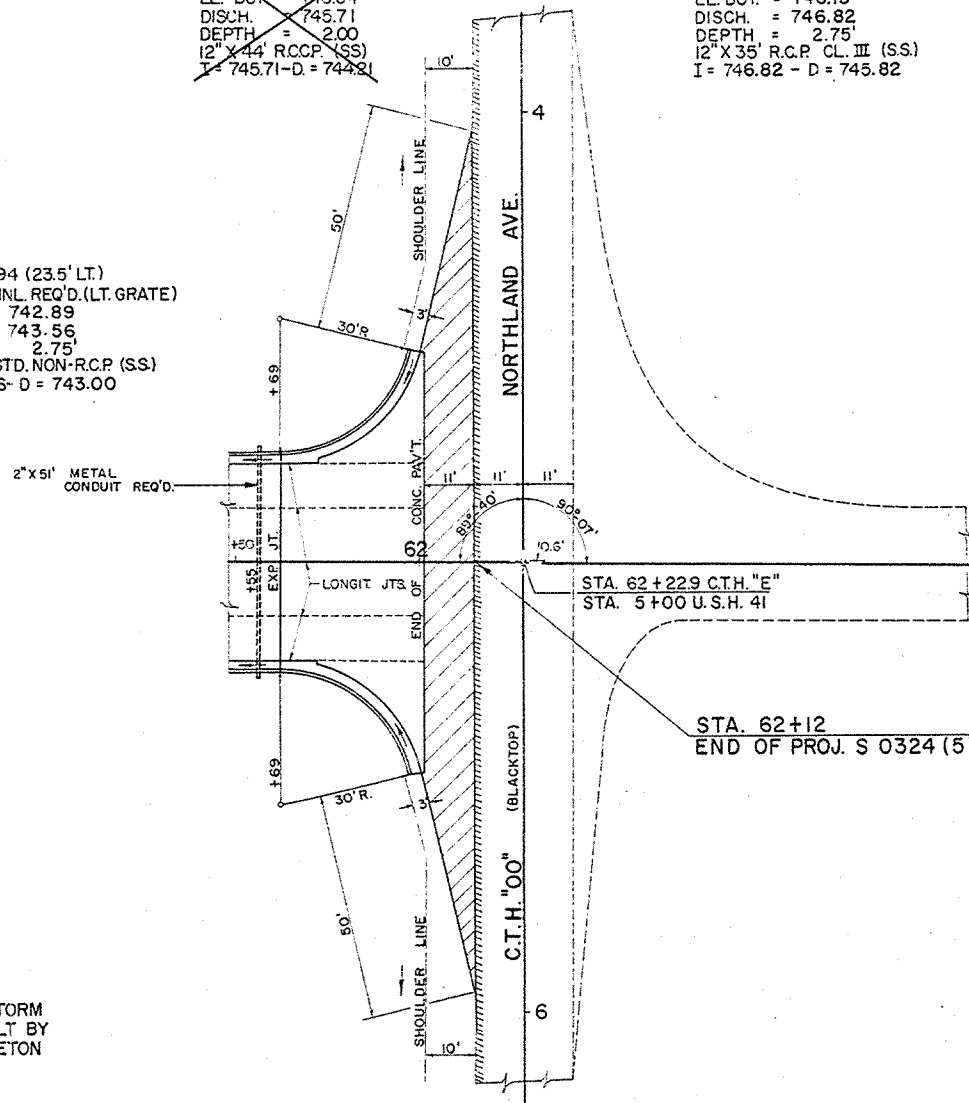
STA. 54+79 (23.5' RT.)
TYPE I-A INL. REQ'D. (LT. GRATE)
EL. BOT. = 746.76
DISCH. = 747.43
DEPTH = 2.75'
12" X 36" R.C.P. CL. III (S.S.)
I = 747.43 - D = 746.50

STA. 59+56 (23.5' RT.)
TYPE 3-H INL. REQ'D. (LT. GRATE)
EL. BOT. = 742.08
DISCH. = 742.75
DEPTH = 3.50'
12" X 17" R.C.P. CL. III (S.S.)
I = 742.75 - D = 742.08



60" STORM SEWER TO BE BUILT BY THE CITY OF APPLETON

NOTE: ELEV'S ARE APPROX.



LEGEND

R.C.P. CL. III (S.S.)	REINFORCED CONCRETE PIPE, CLASS III, STORM SEWER
STD. NON-R.C.P. (S.S.)	STANDARD STRENGTH NONREINFORCED CONCRETE PIPE, STORM SEWER
24"	EXISTING STORM SEWER
12"	PROPOSED STORM SEWER
(Hatched area)	PROPOSED BIT. CONC. PAV'T.

LAYOUTS FOR PROPOSED STORM SEWER AND SIDE STREET INTERSECTIONS

SCALE
0' 5' 10' 20' 40'

ESTIMATE OF QUANTITIES

CONTRACT NO. 1

THIS PROJECT IS TO BE EXECUTED UNDER THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION OF THE STATE HIGHWAY COMMISSION OF WISCONSIN - EDITION OF 1963
 SUBMITTED FOR APPROVAL JAN. 25, 1963 AND SPECIAL PROVISIONS AS ATTACHED TO PROPOSALS

S. H. R. DIVISION	PROJECT	SHEET NO.	TOTAL SHEETS
4	S 0324 (5)	3	33

SEC. NO.	STATION TO STATION	NET LENGTH OF CENTER LINE	CLEARING		GRUBBING		EXCAVATION					CONCRETE			GRAVEL OR CRUSHED STONE		CONCRETE PAVEMENT		CONCRETE DRIVEWAYS	CONCRETE HEADERS	CONCRETE SURFACE DRAINS	CONCRETE PAVEMENT REINF.	PREP. ROAD-BED FOR BITUM. SURF.	BITUM. MAT. FOR PRIME COAT	SINGLE AGGREGATE BITUMINOUS SURFACE	BITUMINOUS CONCRETE PAVEMENT	BITUM. MAT. FOR SURFACE COURSE				
			STA.	IN DIA.	STA.	IN DIA.	REMOVING OLD CULVERT	REMOVING OLD BRIDGE	REMOVING PAVT.	REMOVING CURB & GUTTER	REMOVING CONC. SIDE-WALK	UNCLASSIFIED	ROCK	MARSH	BORROW	GRANULAR BACK-FILL	FINISHING ROADWAY	OBTLITERATING OLD ROAD										GRANULAR SUBBASE COURSE	BASE COURSE	IN STOCK PILES	9"
	ITEM NO.		20102	20105	20301	2104-21	20401	20405	20406	20503				21301		30402	40909	40931	40933	40950	40202	40701	40702								
	UNIT	LIN. FT.	STA.	IN DIA.	STA.	IN DIA.	LS.	LS.	SQ. YD.	LIN. FT.	SQ. YD.	CY.	CY.	CY.	CY.	L.S.	STA.	SQ. YD.	TON	SQ. YD.	SQ. YD.	SQ. YD.	SQ. YD.	CY.	SQ. YD.	STA.	GAL.	TON	TON	TON	
1	10 + 24 - 24 + 24.9	1,373.27						75	54		3,900				0.3		3,116	6,960		50	6,780										
3	34 + 35.2 - 62 + 12	2,776.80	400	400	1			30	234	70	6,050				0.5		5,604	13,890		40	13,580				76	100	6				
	SUB-TOTAL (RURAL)		400	400	1			105	288	70	9,950				0.8		8,720	20,850		40	50	20,360			76	100	6				
	SUB-TOTAL (URBAN)																														
2	24 + 24.9 - 34 + 35.2	1,010.30	130	130				185	12	95	2,450				0.2		2,100	5,150		110	4,940			24	30	2					
	(NO FEDERAL PARTICIPATION IN URBAN SECTION)																														
	PROJ. TOTAL		530	530	1			290	300	165	12,400				1		10,820	26,000		150	50	25,300			100	130	8				

BRIDGES (STRUCTURES OVER 20 FT. SPAN)

CULVERTS (STRUCTURES 20 FT. SPAN & UNDER)

SEC. NO.	REMOVE OLD BRIDGE STA.	CONV. MON. EXC.	EXCAVATION FOR STRUCTURES	GRANULAR BACK-FILL	CONCRETE MASONRY	BAR STEEL REINF.	STRUCTURAL CARBON STEEL	SHEET LEAD	SHEET ZINC	BRIDGES (STRUCTURES OVER 20 FT. SPAN)											CULVERTS (STRUCTURES 20 FT. SPAN & UNDER)																			
										TREATED TIMBER		CONC. TEST		CONCRETE PILING		CAST-IN-PLACE CONC. TEST		FLOOR DRAINING	RIP-RAP	SLOPE PAVING	REMOVE OLD CULVERT STA.	CONV. MON. EXC.	EXCAVATION FOR STRUCTURES	GRANULAR BACK-FILL	CONCRETE MASONRY	BAR STEEL REINF.	STRUCTURAL CARBON STEEL	SHEET LEAD	SHEET ZINC	TREATED TIMBER		CONC. TEST		CONCRETE PILING		CAST-IN-PLACE CONC. TEST		FLOOR DRAINING	RIP-RAP	SLOPE PAVING
UNTR'D DELIVERED	TR'D DELIVERED	UNTR'D DELIVERED	TR'D DELIVERED	UNTR'D DELIVERED	TR'D DELIVERED	UNTR'D DELIVERED	TR'D DELIVERED	UNTR'D DELIVERED	TR'D DELIVERED	UNTR'D DELIVERED	TR'D DELIVERED	UNTR'D DELIVERED	TR'D DELIVERED	UNTR'D DELIVERED	TR'D DELIVERED	UNTR'D DELIVERED	TR'D DELIVERED													UNTR'D DELIVERED	TR'D DELIVERED	UNTR'D DELIVERED	TR'D DELIVERED	UNTR'D DELIVERED	TR'D DELIVERED	UNTR'D DELIVERED	TR'D DELIVERED			
	2104-21	2105-1	2107-1	2110-1	2404-1	2408-1	2407-1	2407-9	2407-2	2408-2	2501-1	2501-2	2501-3	2501-4	2501-5	2502-1	2502-2	2502-3	2515-1	2505-2	2505-3	2508-1	2512-1	2515-1	2104-1	2105-1	2107-2	2110-2	2404-1	2405-2	2407-1	2407-9	2407-11	2408-2	2501-1	2501-4	2501-5	2508-1	2512-1	
	LS.	CY.	CY.	CY.	LS.	LS.	LB.	LB.	LB.	LS.	LS.	LS.	LS.	LS.	LS.	LS.	LS.	LS.	LS.	LS.	LS.	LS.	LS.	LS.	LS.	CY.	CY.	CY.	CY.	LB.	LB.	LB.	LB.	LS.	LS.	LS.	LS.	LS.	LS.	CY.

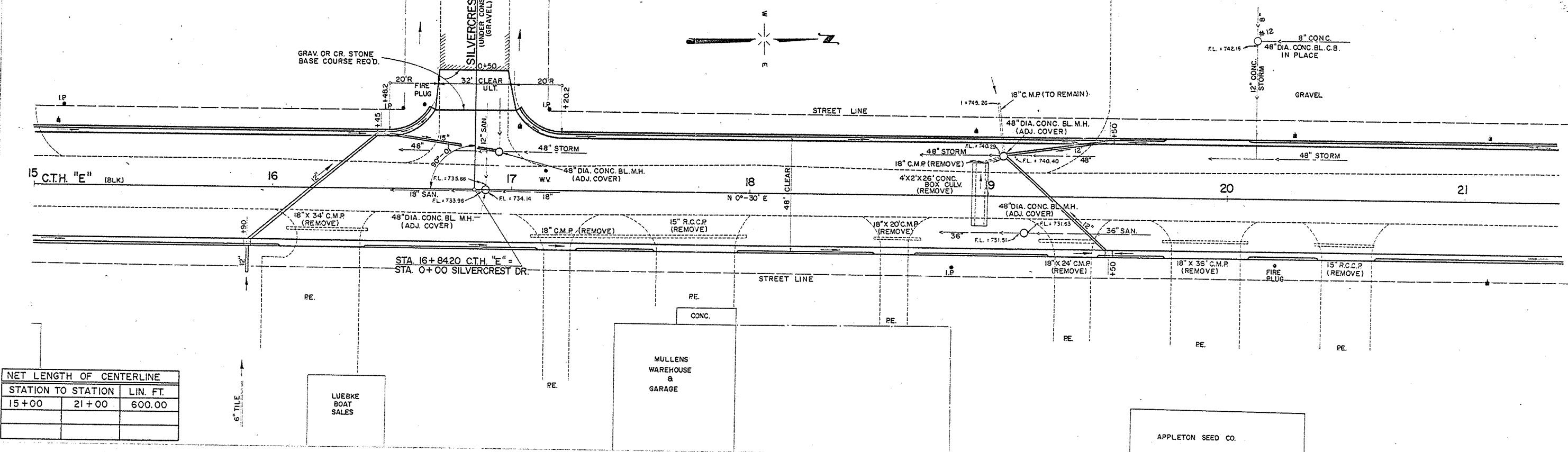
COVERS

SEC. NO.	RUSSLE MASONRY		MANHOLES				RIP-RAP	HEAVY RIP-RAP	SLOPE PAVING		DITCH CHECKS	STORM			SEWER			MANHOLES			INLETS			CONCRETE			STEEL PLATE BEAM GUARD	MARKER POSTS FOR R.O.W.	ANCHOR PAGES FOR CABLE GUARD FENCE	CALCIUM CHLORIDE SURFACE TREATMENT	TOP-SOIL	SALVAGED TOP-SOIL	FER-TILIZER	SEEDING	SOO-DING	PIPE RAILING	LANDMARK MONUMENTS	2" METAL CONDUIT
	MORTAR	DRY	TYPE J	TYPE	TYPE A	TYPE D			TYPE H.	10"		12"	15"	NON-R.C.P.	R.C.P.	CL.II	CL.III	TYPE 1	TYPE 2	TYPE 3	30" CURB AND GUTTER TYPE 'A'	30" CURB AND GUTTER TYPE 'B'	SIDE WALKS 5'	STEPS	TYPE 1	TYPE 2												
	51801		61151	61161	61167			60703	60825	60826	61111	61113	61182	61121	90001	60123	60133	60205	60215	62301	62501	62505	62901	63001	63101	51301	62101	61302										
	CY.	CY.	LIN. FT.	LIN. FT.	EACH	EACH	EACH	CY.	CY.	SQ. YD.	SQ. YD.	CY.	LIN. FT.	LIN. FT.	LIN. FT.	LIN. FT.	LIN. FT.	LIN. FT.	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH
1			1	3	4			82	210	46		1	11	3	4	2,671	29			3	1,650	1,650	3.8	4,250	10		14	52										
3				14	8			137	492	58			21	14	8	5,391	208	425		6	3,200	3,200	7.5	8,250	10		10	51										
	SUB-TOTAL (RURAL)		1	17	12			219	702	104		1	32	17	12	8,062	237	425		9	4,850	4,850	11.3	12,500	20		24	103										
	SUB-TOTAL (URBAN)		2	4	6			317	162			1	10	5	6	1,958	38	565	28		2	1,150	1,150	2.7	3,000	30	6	2										
	PROJ. TOTAL		2	21	18			219	1,019	266		1	42	22	18	10,020	275	990	28		11	6,000	6,000	14.0	15,500	50	6	26	103									

TELEPHONE CO. S 0324(5) 5 33

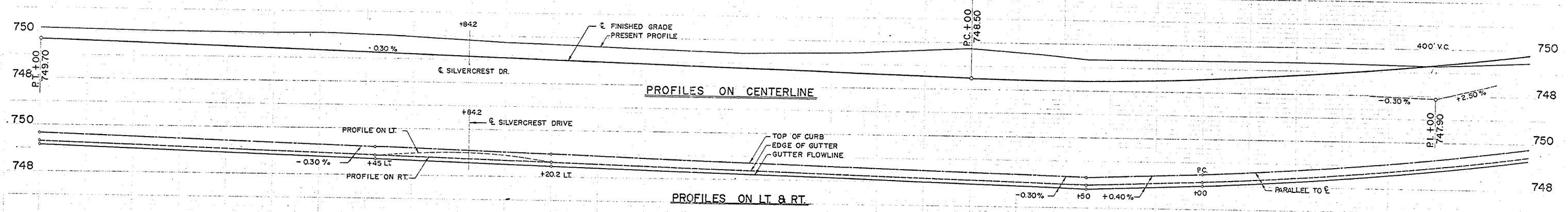
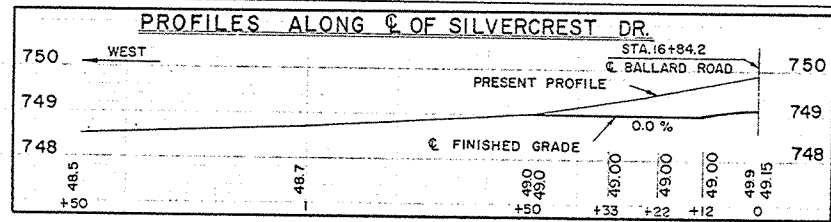
BENCH MARKS

NO.	STATION	DESCRIPTION	ELEV.
2	16+63	ARROW ON HYDRANT 36' LT.	751.92
3	20+18	" " " 27' RT.	750.66



NET LENGTH OF CENTERLINE

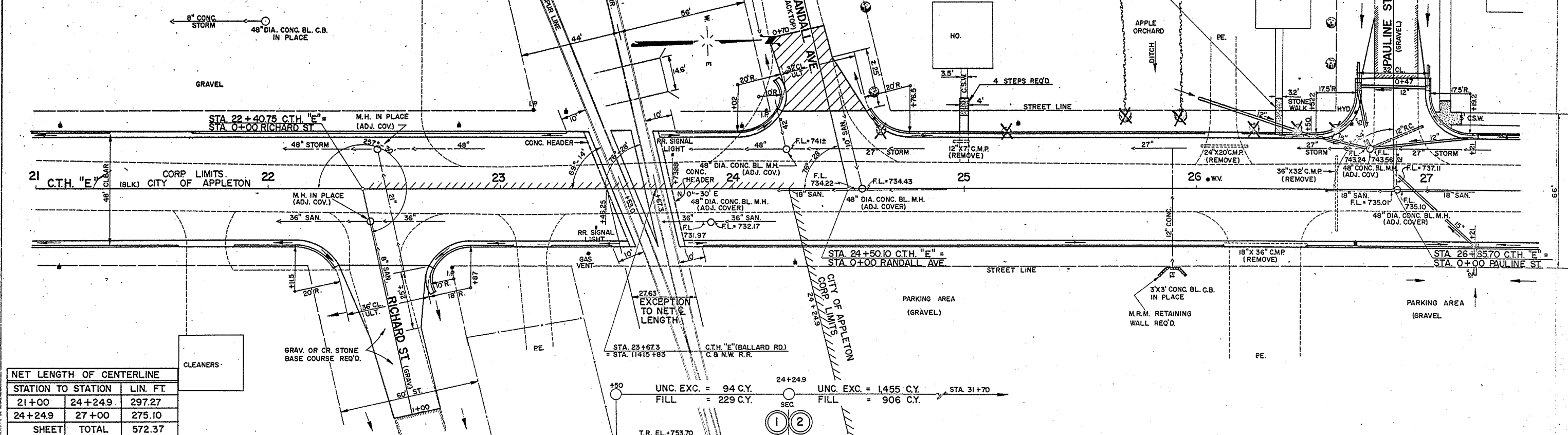
STATION TO STATION	LIN. FT.	
15+00	21+00	600.00



50 15	50 05	50 05	49 26	49 03	49 03																		
49 70	49 55	49 40	48 76	48 53	48 53																		
49 32	49 17	49 05 49 02	48 89	48 66	48 66																		
49 19	49 04	49 02 48 87	49 27 49 25	49 04	49 04																		
49 69	49 54	48 92 48 89	48 77 48 72	48 95	48 95																		
		49 42 49 39	48 74 48 59	48 80	48 80																		
			49 14 49 09	48 64	48 64																		
				49 14	49 09																		
15	+50	+90	16	+45	+90	+842	17	+202	+50	18	+50	19	+50	20	+50	20	+50	20	+50	21	+50	21	

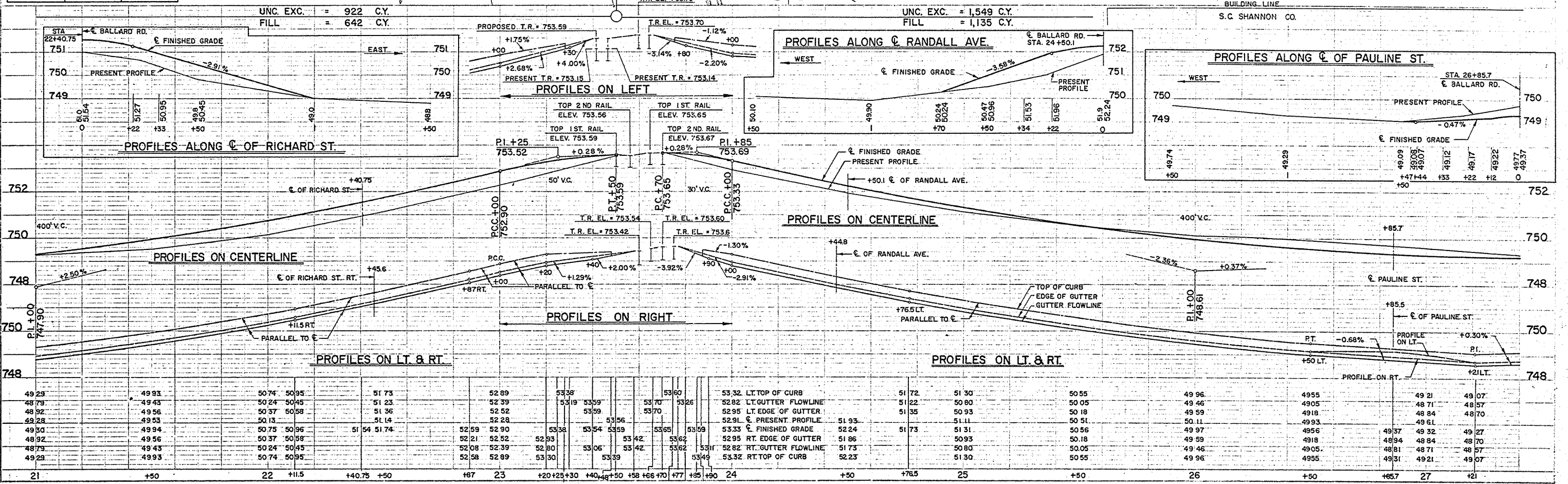
BENCH MARKS

NO.	STATION	DESCRIPTION	ELEV.
4	23+51	PT MK NW COR. SIGNAL BOX 85' RT.	752.61
5	26+70	ARROW ON HYDRANT 33' LT.	750.93



NET LENGTH OF CENTERLINE

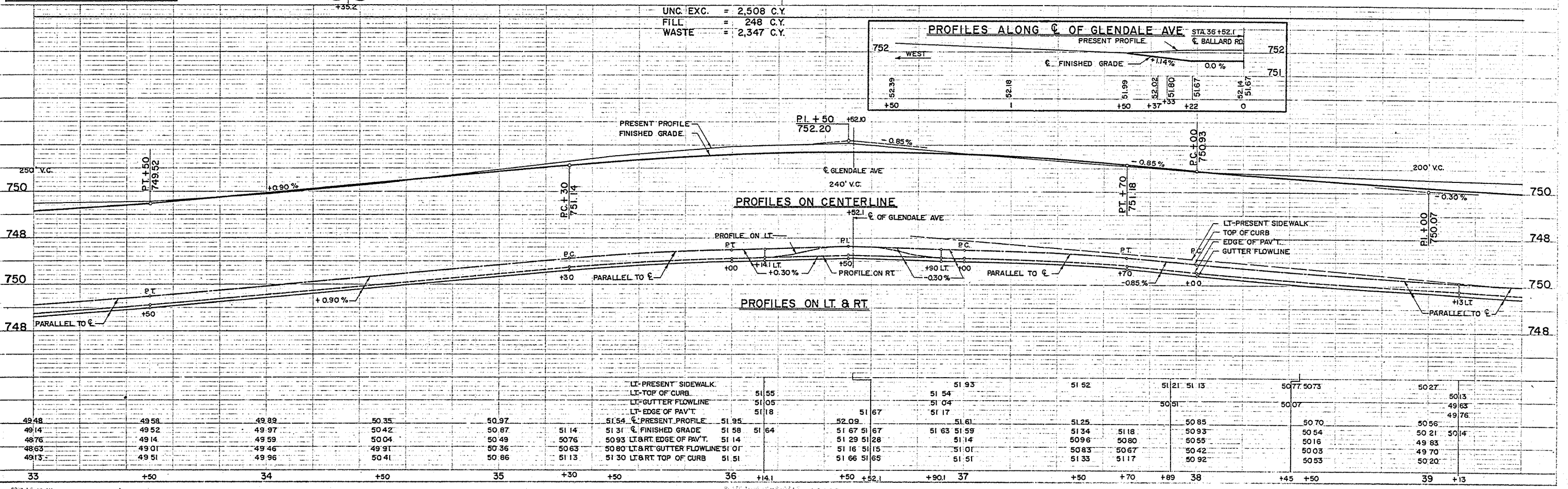
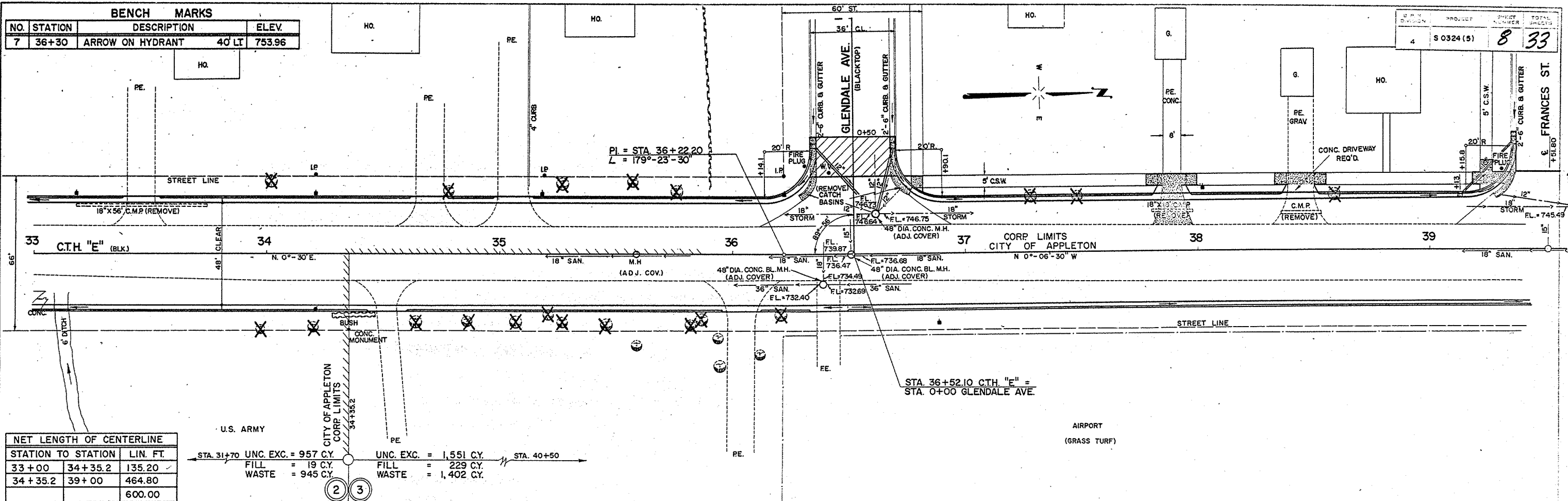
STATION TO STATION	LIN. FT.	
21+00	24+24.9	297.27
24+24.9	27+00	275.10
SHEET TOTAL		572.37



21	+50	22	+11.5	+40.75	+50	+67	23	+20	+25	+30	+40	+48	+50	+58	+66	+70	+77	+85	+90	24	+50	+76.5	25	+50	26	+50	+65.7	27	+21
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DATE: 10-61
 BY: L.L.B.
 CHECKED: L.L.B.
 NO. 389

DATE: 10-61
 BY: L.L.B.
 CHECKED: L.L.B.
 NO. 390



DATE: 12-81
 12-81
 1-82
 4-82
 5-82

BY: L.L.B.
 L.L.B.
 L.L.B.

REVISIONS:
 1. L.L.B.
 2. L.L.B.
 3. L.L.B.

PROJECT: S 0324 (5)
 SHEET: 8
 TOTAL SHEETS: 33

DATE: 10-81
 11-81
 4-82
 4-82

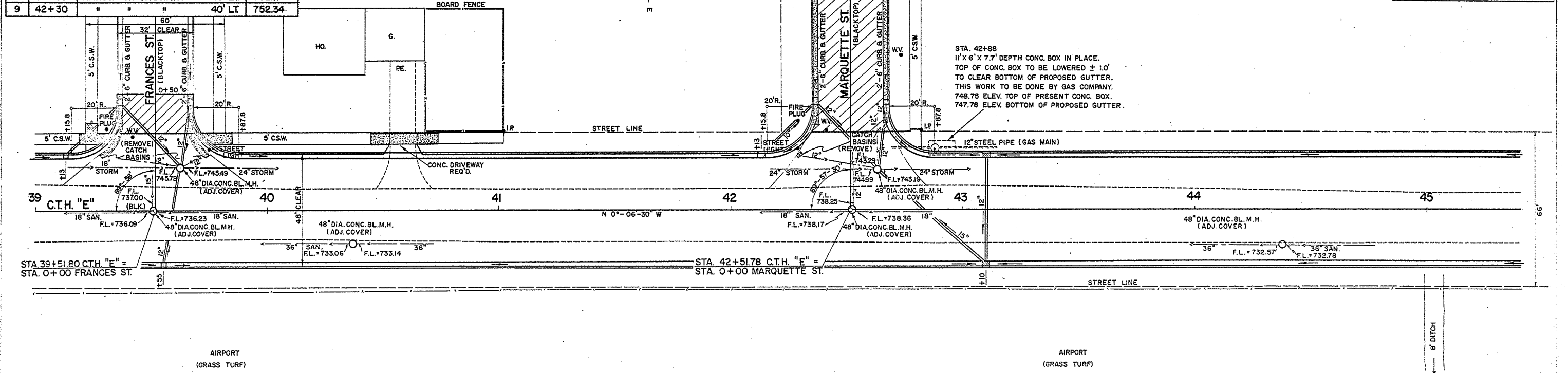
BY: L.L.B.
 L.L.B.
 L.L.B.

REVISIONS:
 1. L.L.B.
 2. L.L.B.
 3. L.L.B.

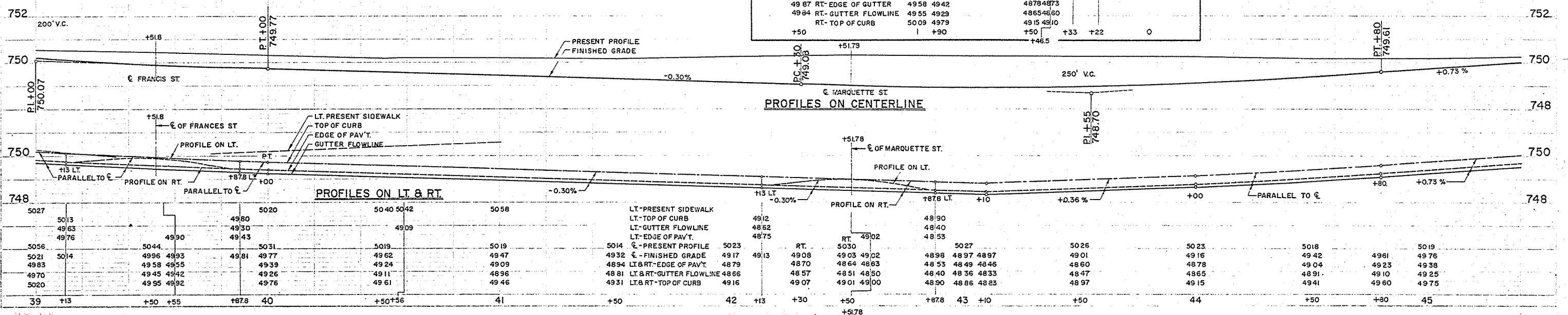
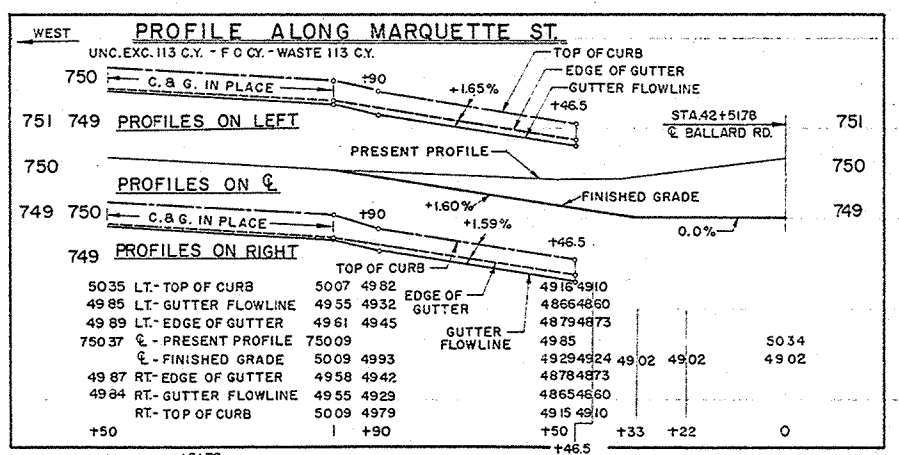
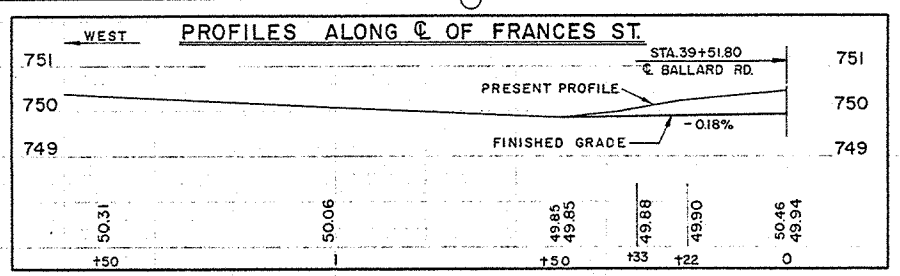
PROJECT: S 0324 (5)
 SHEET: 8
 TOTAL SHEETS: 33

BENCH MARKS			
NO.	STATION	DESCRIPTION	ELEV.
8	39+30	ARROW ON HYDRANT 40' LT	752.17
9	42+30	" " " 40' LT	752.34

PROJECT	NO.	TOTAL SHEETS
S0324 (5)	9	33



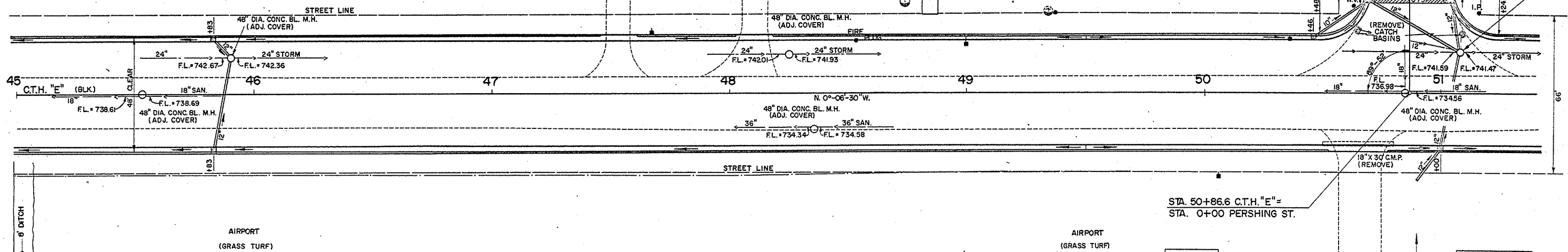
NET LENGTH OF CENTERLINE		
STATION TO STATION	LIN. FT.	
39+00	45+00	600.00



STATION	LT. PRESENT SIDEWALK	LT. TOP OF CURB	LT. GUTTER FLOWLINE	LT. EDGE OF PAV'T.	RT. TOP OF CURB	RT. GUTTER FLOWLINE	RT. EDGE OF PAV'T.	FINISHED GRADE
39+00	5027	5013	4963	4976	5007	4985	4998	750.07
40+00	5020	4980	4930	4943	5020	4988	4999	750.00
41+00	5040	5042	4909	4911	5019	4947	4977	749.95
42+00	5058	5019	4909	4926	5019	4947	4977	749.95
43+00	5014	4932	4894	4881	5014	4932	4894	749.95
44+00	5026	4901	4860	4878	5026	4901	4860	749.95
45+00	5023	4916	4865	4891	5023	4916	4865	749.95

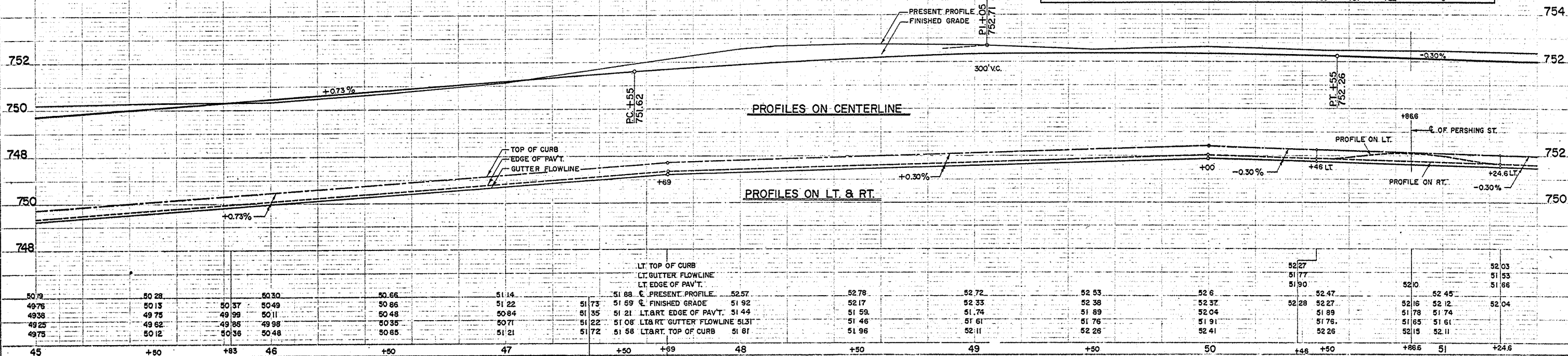
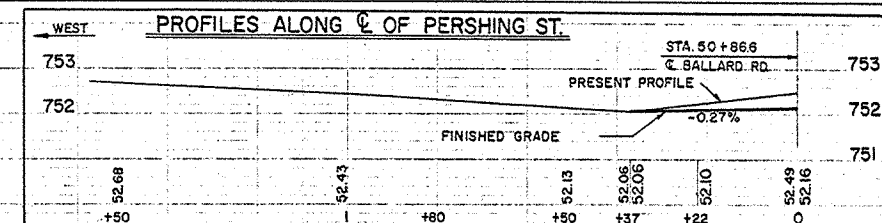
BENCH MARKS			
NO.	STATION	DESCRIPTION	ELEV.
10	48+53	ARROW ON HYDRANT	754.24

PROJECT: S 0324 (5)
SHEET: 10
TOTAL SHEETS: 33



NET LENGTH OF CENTERLINE		
STATION TO STATION	LIN. FT.	
45 + 00	51 + 00	600.00

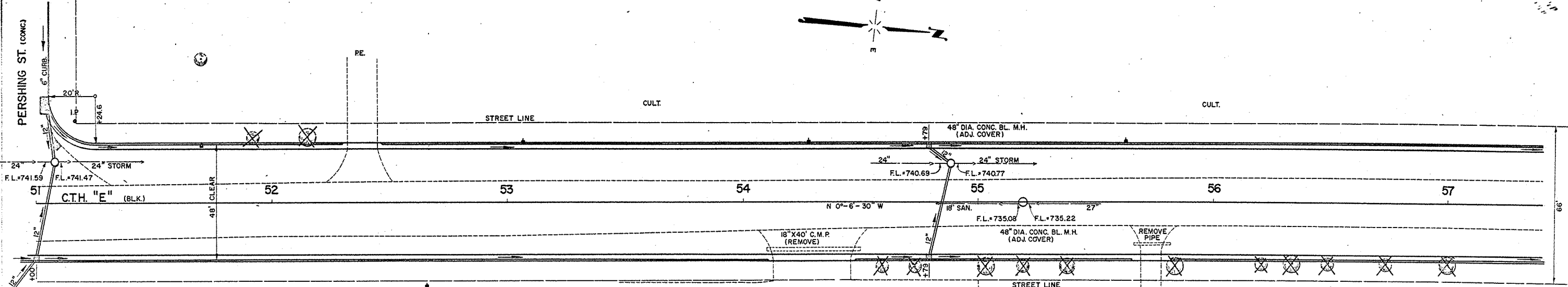
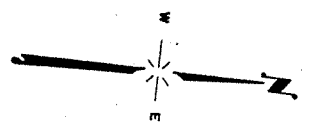
STA. 40+50 TO STA. 62+12
UNC. EXC. = 4,370 CY.
FILL = 3,446 CY.



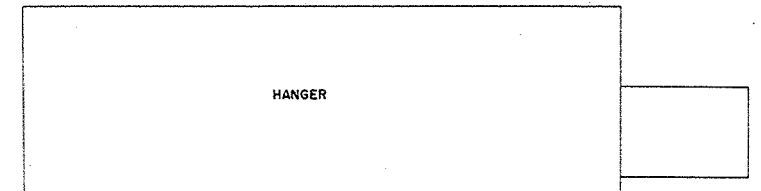
STATION	LT. TOP OF CURB	LT. GUTTER FLOWLINE	LT. EDGE OF PAVT.	PRESENT PROFILE	FINISHED GRADE	LT. & RT. EDGE OF PAVT.	LT. & RT. GUTTER FLOWLINE	LT. & RT. TOP OF CURB
45	50.19	49.75	49.38	49.25	49.75	50.12	50.36	50.48
+50	50.28	49.86	49.51	49.38	49.86	50.12	50.36	50.48
+83	50.37	49.95	49.60	49.47	49.95	50.21	50.45	50.57
46	50.49	50.04	49.69	49.54	50.04	50.30	50.54	50.66
+50	50.66	50.22	49.87	49.72	50.22	50.51	50.75	50.87
47	51.14	50.70	50.35	50.20	50.70	51.00	51.24	51.36
+50	51.22	50.78	50.43	50.28	50.78	51.08	51.32	51.44
+69	51.55	51.11	50.76	50.61	51.11	51.41	51.65	51.77
48	51.88	51.44	51.09	50.94	51.44	51.74	51.98	52.10
+50	51.69	51.25	50.90	50.75	51.25	51.55	51.79	51.91
49	52.78	52.34	51.99	51.84	52.34	52.64	52.88	53.00
+50	52.72	52.28	51.93	51.78	52.28	52.58	52.82	52.94
50	52.53	52.09	51.74	51.59	52.09	52.39	52.63	52.75
+46	52.6	52.16	51.81	51.66	52.16	52.46	52.70	52.82
+50	52.37	51.93	51.58	51.43	51.93	52.23	52.47	52.59
+86.6	52.27	51.83	51.48	51.33	51.83	52.13	52.37	52.49
51	52.03	51.59	51.24	51.09	51.59	51.89	52.13	52.25
+24.6	51.53	51.09	50.74	50.59	51.09	51.39	51.63	51.75

BENCH MARKS		HO.	
NO.	STATION	DESCRIPTION	ELEV.
11	54+58	SPIKE IN 12" ASH	27' RT. 752.13

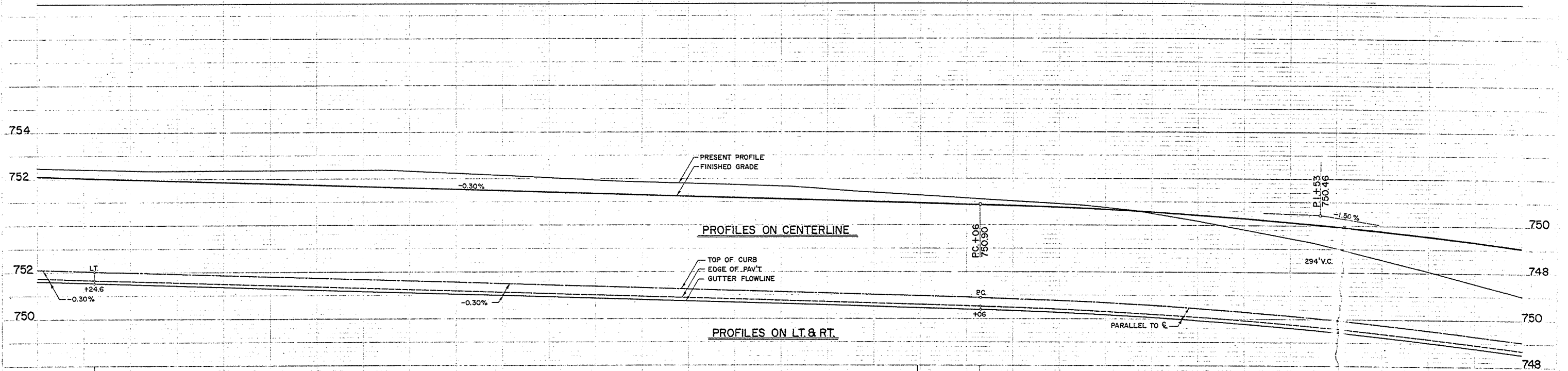
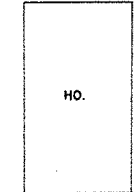
PROJECT	SHEET NUMBER	TOTAL SHEETS
S 0324 (5)	11	33



NET LENGTH OF CENTERLINE		
STATION TO STATION	LIN. FT.	
51+00	57+00	600.00



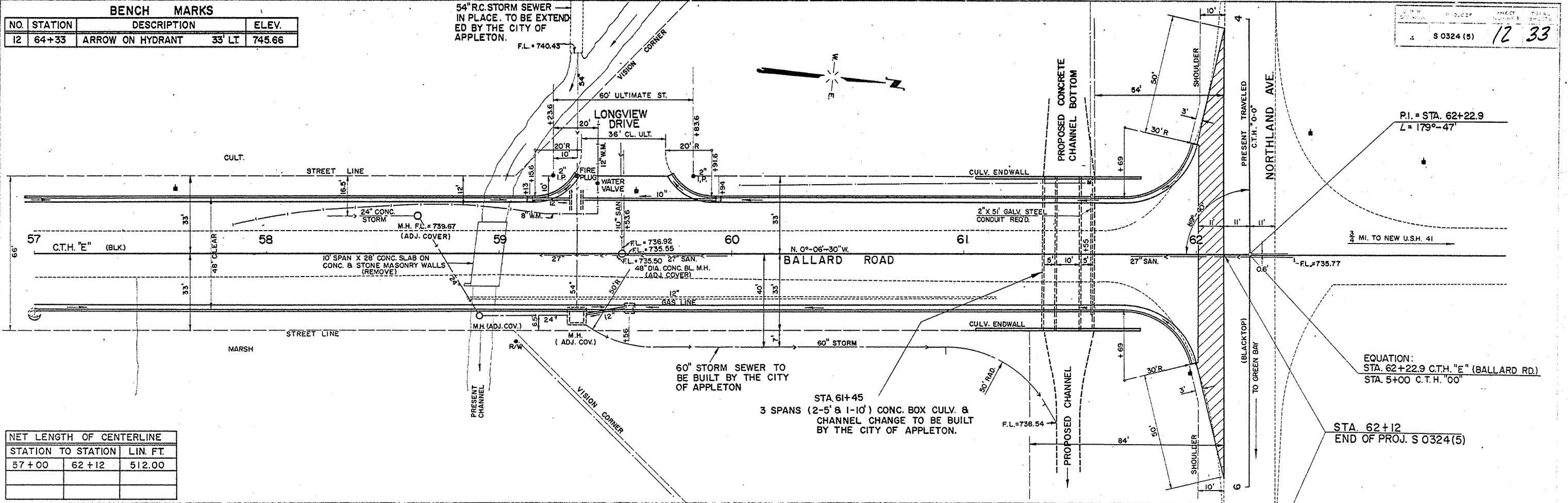
AIRPORT PARKING LOT (GRAVEL)



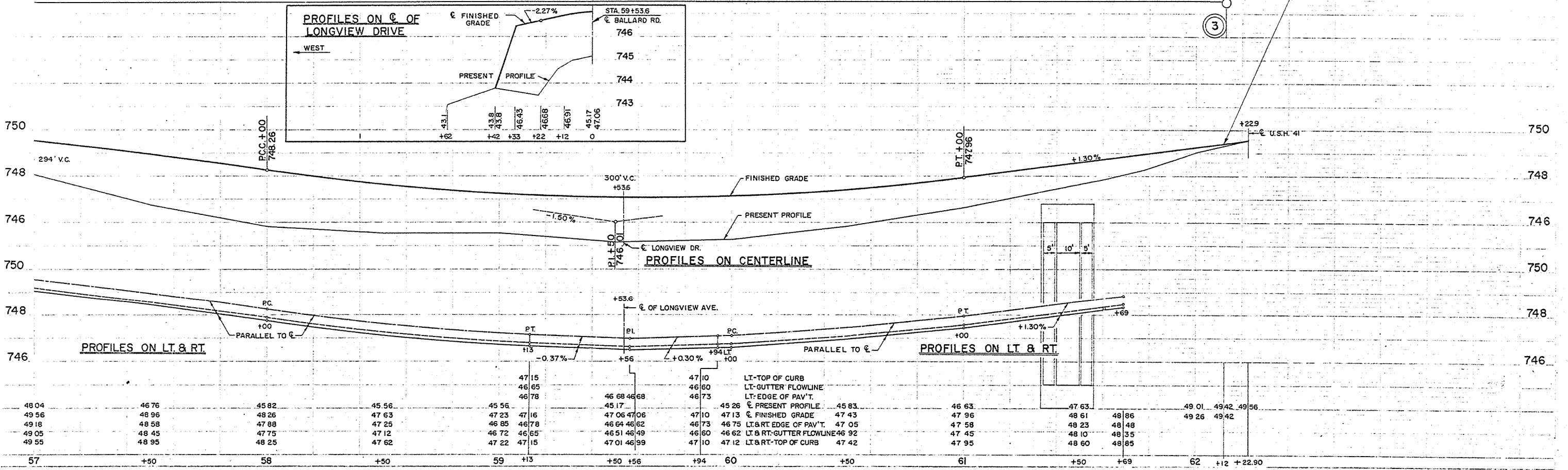
STATION	LT. TOP OF CURB	LT. GUTTER FLOWLINE	LT. EDGE OF PAV'T.	PRESENT PROFILE	FINISHED GRADE	RT. TOP OF CURB	RT. GUTTER FLOWLINE	RT. EDGE OF PAV'T.
51	52.03	51.53	51.66	51.75	51.22	51.74	51.61	51.61
52	52.35	51.97	51.46	52.03	51.48	52.12	51.61	51.61
53	52.14	51.82	51.01	52.14	51.48	52.12	51.61	51.61
54	51.89	51.37	50.86	51.75	51.22	51.74	51.61	51.61
55	51.15	50.92	50.41	51.15	50.68	51.15	50.92	50.92
56	50.83	50.73	50.39	50.83	50.73	50.83	50.73	50.73
57	49.26	49.05	49.54	49.26	49.05	49.26	49.05	49.05

BENCH MARKS				
NO.	STATION	DESCRIPTION		ELEV.
12	64+33	ARROW ON HYDRANT	33' LT.	745.66

S 0324 (5) 12 33



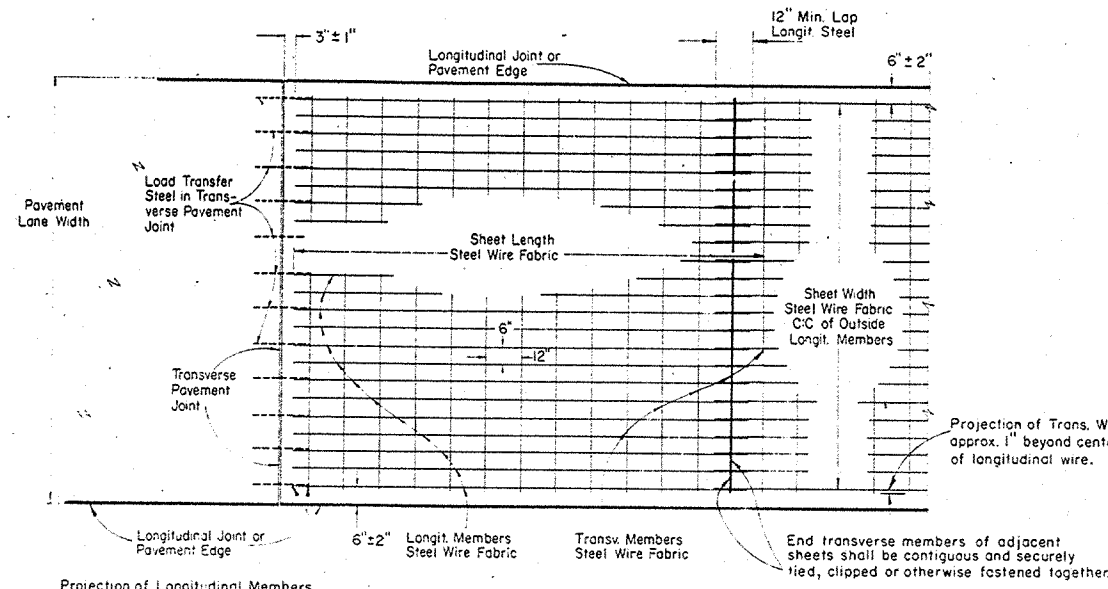
NET LENGTH OF CENTERLINE		
STATION TO STATION	LIN. FT.	
57+00	62+12	512.00



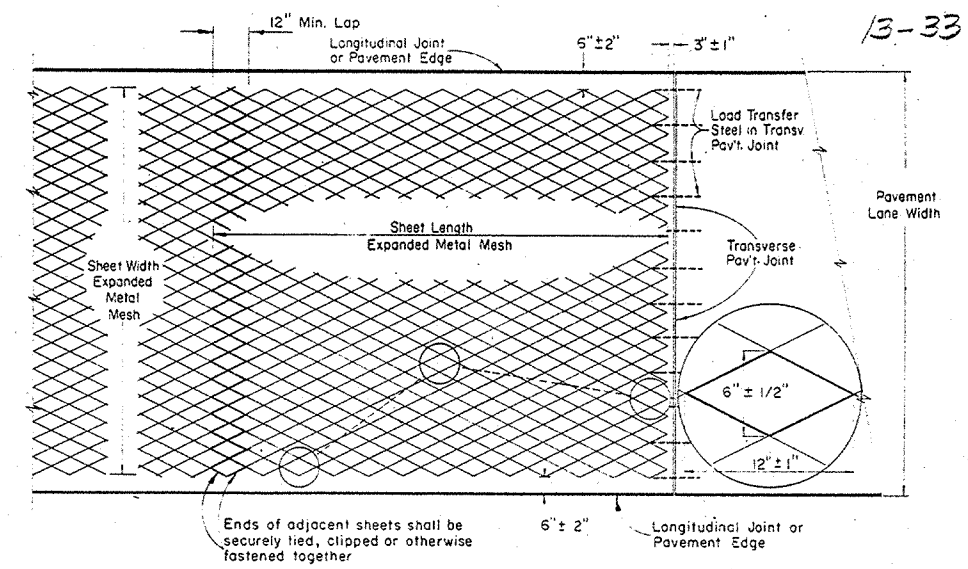
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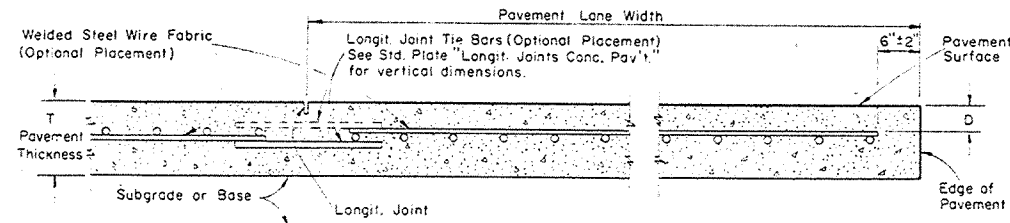
57 +50 58 +50 59 +13 +50 +56 +94 60 +50 61 +50 +69 62 +12 +22.90



PLAN VIEW



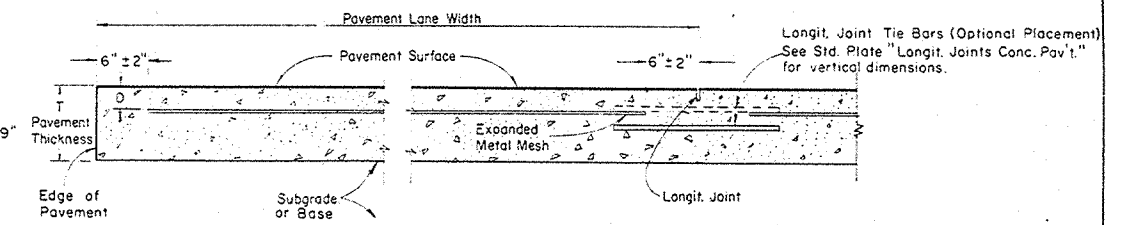
PLAN VIEW



CROSS SECTION

WELDED STEEL WIRE FABRIC

D = 2" to 3" for T = 8"
 D = 2 1/2" to 3 1/2" for T = 9"
 D = 3" to 4" for T = 10"



CROSS SECTION

EXPANDED METAL MESH

GENERAL NOTES -

Details of Construction and Materials not shown hereon shall conform to the pertinent requirements of the Standard Specifications and the applicable Special Conditions.

WELDED STEEL WIRE FABRIC

Manufacturers No. 612-04
 Approx. Weight per 100 Sq. Ft. = 69.0 lbs.
 Longitudinal Steel - Gage No. 0 = 0.3065" D. at 6" C.C.
 Transverse Steel - Gage No. 4 = 0.2253" D. at 12" C.C.

Welded Steel Wire Fabric shall conform to the requirements of the Standard Specifications for Welded Steel Wire Fabric for Concrete Reinforcement A.A.S.H.O. Designation M55.
 Side lap of adjacent sheets approximate 6"

SHIPPING REQUIREMENTS

Welded Steel Wire Fabric or Expanded Metal Mesh Concrete Pavement Reinforcement shall be shipped to the job site in flat sheets.

EXPANDED METAL MESH

Weight per 100 Sq. Ft. = 76.0 lbs. min.
 Expanded Metal Mesh shall be manufactured from open hearth steel, having a phosphorus content of not more than 0.05 percent, and a yield point of not less than 55,000 p.s.i. The steel shall be sufficiently ductile to permit any strand to be bent through an angle of 180 degrees over one diam. without fracture. The diamond shaped mesh shall be fabricated by a cold drawn process which will cut and draw the steel forming uniform dimensioned strands conforming to shape and weight as shown elsewhere hereon.
 Side lap of adjacent sheets approximate 6"

CONCRETE PAVEMENT REINFORCEMENT

STATE HIGHWAY COMMISSION OF WISCONSIN

RECOMMENDED FOR APPROVAL:

2-5-63
 DATE

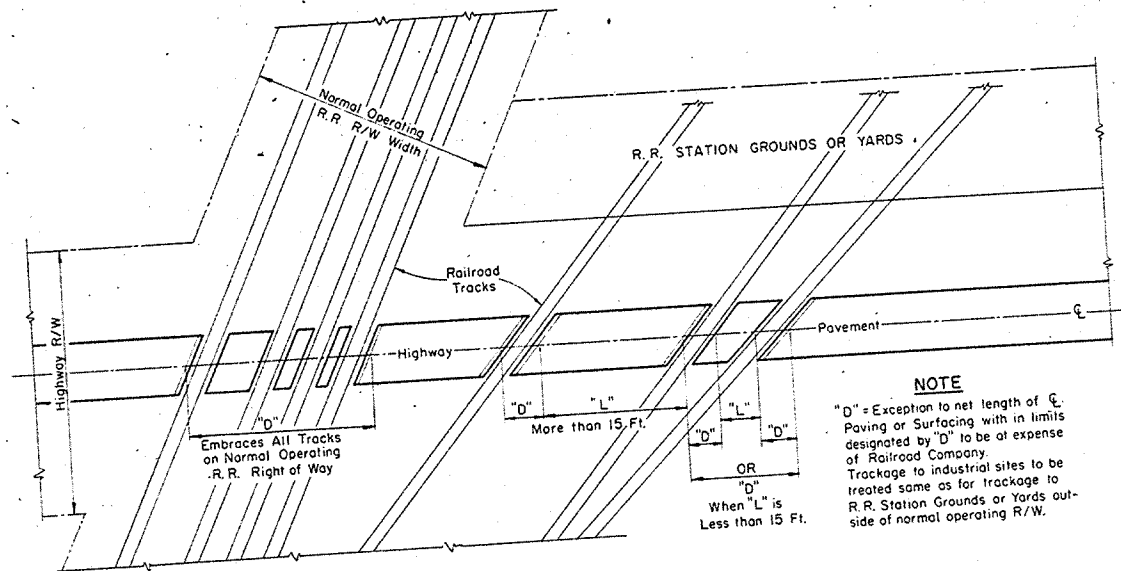
J. L. Pelly
 ENGINEER OF DESIGN

APPROVED:

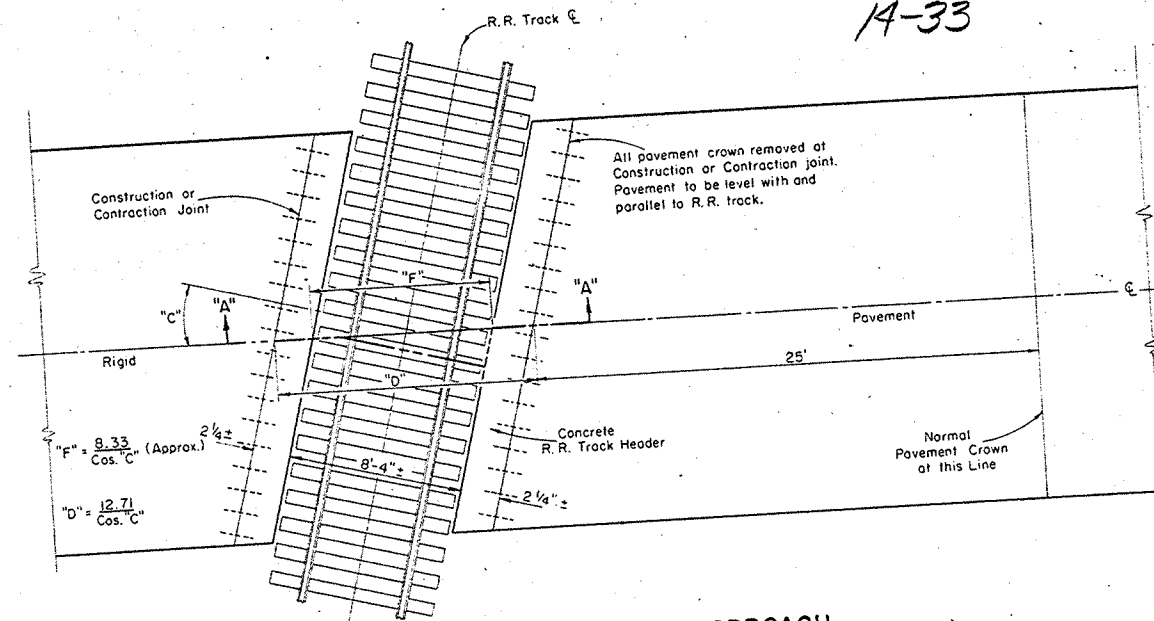
2/6/63
 DATE

E. C. Rottman
 STATE HIGHWAY ENGINEER

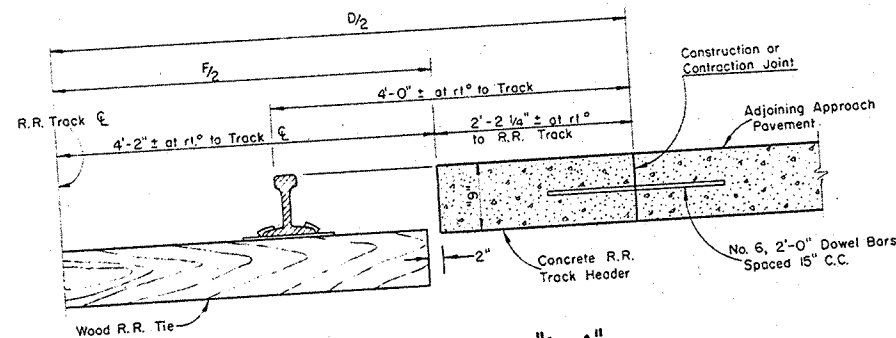
PLATE NO. 2-1.1.11



**TYPICAL TYPES OF RAILROAD GRADE CROSSINGS
SHOWING PAVEMENT HEADER LOCATIONS
AND MEASUREMENT DETAILS**



**RAILROAD APPROACH
CONSTRUCTION DETAILS**



**CROSS SECTION "A-A"
SHOWING PAVEMENT HEADER DETAILS
AT R.R. TRACKS**

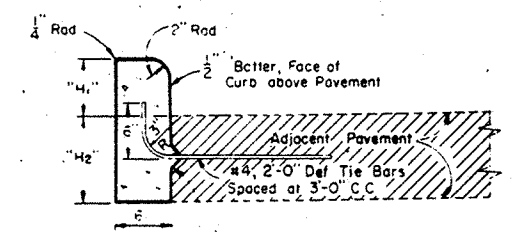
GENERAL NOTES

Details of construction not shown on this drawing shall conform to the pertinent requirements of the Standard Specifications and the applicable Special Provisions. The above details apply for concrete surfacing. For bituminous surfacing, construction work shall be carried up to railroad tie ends. Dimension "D" is an exception to Net Centerline Length for all Surfacing and Grading Projects.

**RAILROAD APPROACH DETAILS
CONCRETE PAVEMENT**

STATE HIGHWAY COMMISSION OF WISCONSIN
RECOMMENDED FOR APPROVAL
DATE 2-5-63
APPROVED: J. J. Piff
ENGINEER OF DESIGN
DATE 2/16/63
P. C. Rosation
STATE HIGHWAY ENGINEER
PLATE NO. 2-2.1.10

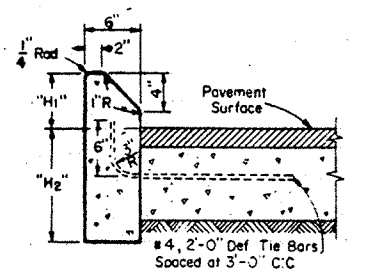
15-33



Tie Bar recess positioned in reverse when Concrete Curb is constructed first.
 $H_1 = 9"$ max and $3\frac{1}{2}"$ min, and shall be 6" unless otherwise shown on the plans.
 $H_2 =$ Same as adjacent pavement thickness for rigid pavement and 12" for other than rigid pavement (Tie Bars Omitted).

TYPE "A" (Including Tie Bars) TYPE "D" (Excluding Tie Bars)

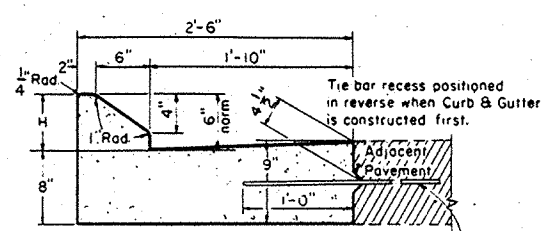
CONCRETE CURB



$H_1 = 9"$ Max and 4" min and shall be 6" unless otherwise shown on plans.
 $H_2 =$ Same as adjacent pavement thickness for rigid pavement and 12" for other than rigid pavement (Tie Bars Omitted).

TYPE "G" (Including Tie Bars) TYPE "J" (Excluding Tie Bars)

CONCRETE CURB (Mountable Type)

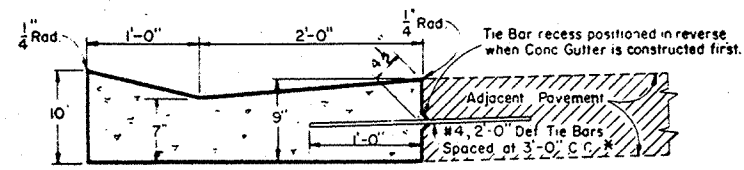


$H_1 = 9"$ max and 4" min & shall be 6" unless otherwise shown on the plans.
 #4, 2-0" Def Tie Bars or alternate Bolt Type instal. may be used, spaced at 3-0" C.C.

TYPE "G" (Including Tie Bars) TYPE "J" (Excluding Tie Bars)

CONCRETE CURB AND GUTTER (Mountable Type)

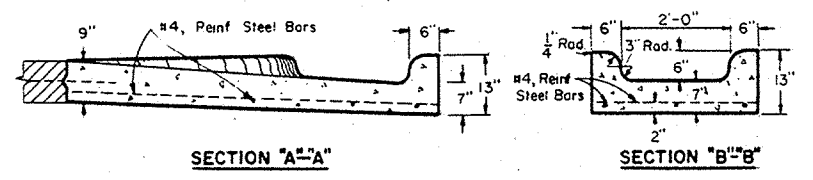
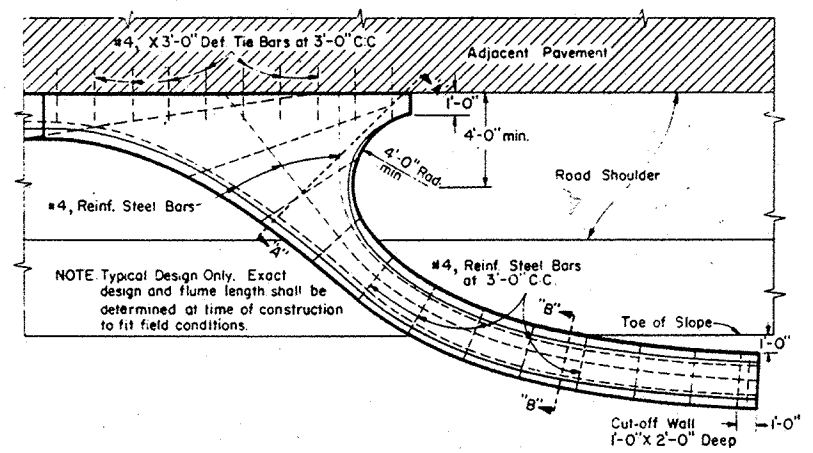
GENERAL NOTES
 Details of construction and materials not shown on this drawing shall conform to the pertinent requirements of the Standard Specifications and the applicable Special Provisions.
JOINTS -
 Joints shall not be sealed in concrete curb, concrete gutter, concrete curb and gutter, or concrete surface drains.



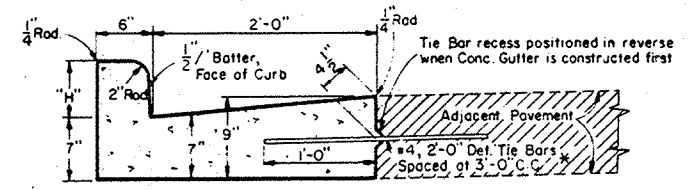
* Alternate Tie Bars or Bolt Type installations may be used as shown for Longitudinal Joints.

TYPE "A" (Including Tie Bars) TYPE "D" (Excluding Tie Bars)

CONCRETE GUTTER



SECTION "A-A" SECTION "B-B"
 CONCRETE INLET OR DISCHARGE FOR CURB AND GUTTER SURFACE DRAIN



$H = 9"$ Max., $3\frac{1}{2}"$ Min., and shall be 6" unless otherwise shown on the plans.
 * Alternate Tie Bars or Bolt Type installations may be used as shown for Longitudinal Joints.

TYPE "A" (Including Tie Bars) TYPE "D" (Excluding Tie Bars)

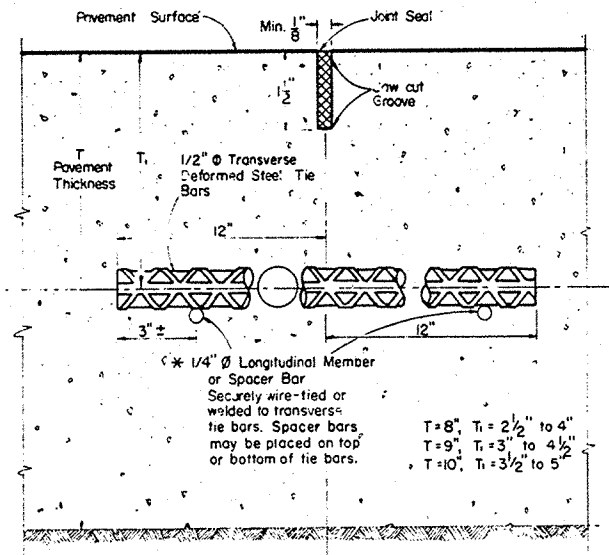
CONCRETE CURB AND GUTTER (Barrier Type)

**CONCRETE CURB, CONCRETE GUTTER
 CONCRETE CURB AND GUTTER AND
 CONCRETE SURFACE DRAINS**

STATE HIGHWAY COMMISSION OF WISCONSIN

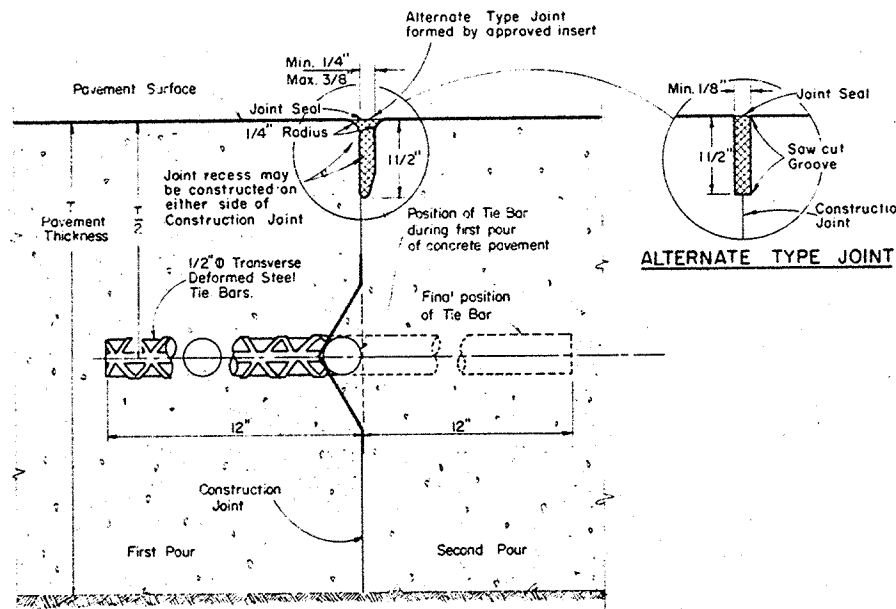
RECOMMENDED FOR APPROVAL
 DATE 2-5-63
 APPROVED: J. S. Pelt ENGINEER OF DESIGN
 DATE 2/6/63
 APPROVED: E. C. Rutledge STATE HIGHWAY ENGINEER

PLATE NO. 3-1.1.7



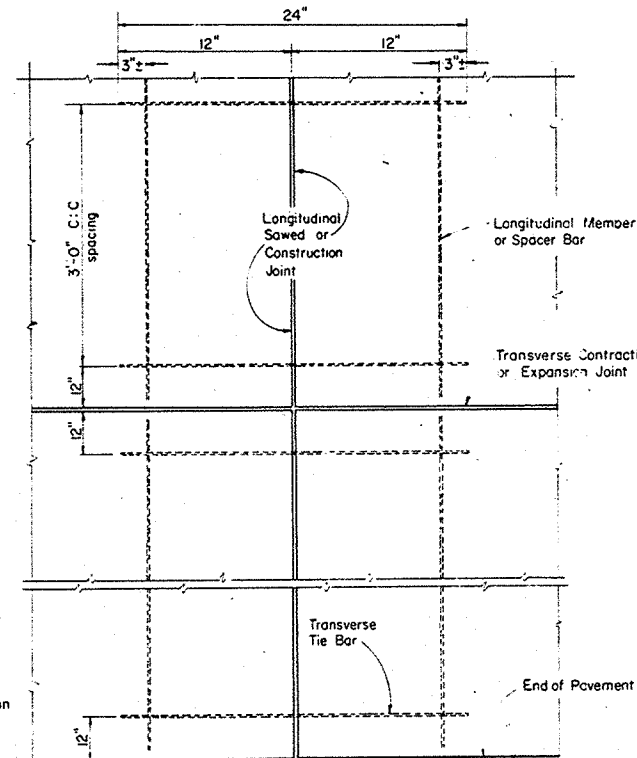
SAWED JOINT

ELEVATION, Showing Tie Bars and Positioning Details



CONSTRUCTION JOINT

ELEVATION, Showing Tie Bars and Positioning Details



PLAN VIEW, Showing Tie Bars and Location Details

16-33

GENERAL NOTES

Details of Construction not shown on this drawing shall conform to the pertinent requirements of the Standard Specifications and the applicable Special Provisions.

ALTERNATE DESIGNS

Alternate designs of Bolt Type joint installations may be used upon written approval of the Engineer.

SEALING JOINTS

All joints shall be sealed.

***TIE BARS**

Tie bars shall be installed as shown (assembled ladder type pattern), or longitudinal member spacer bars may be omitted if other means satisfactory to the Engineer are provided to install and retain tie bars in their proper designated position.

**LONGITUDINAL JOINTS
CONCRETE PAVEMENT**

STATE HIGHWAY COMMISSION OF WISCONSIN

RECOMMENDED FOR APPROVAL:

2-5-63
DATE

J. L. Pitt
ENGINEER OF DESIGN

APPROVED:

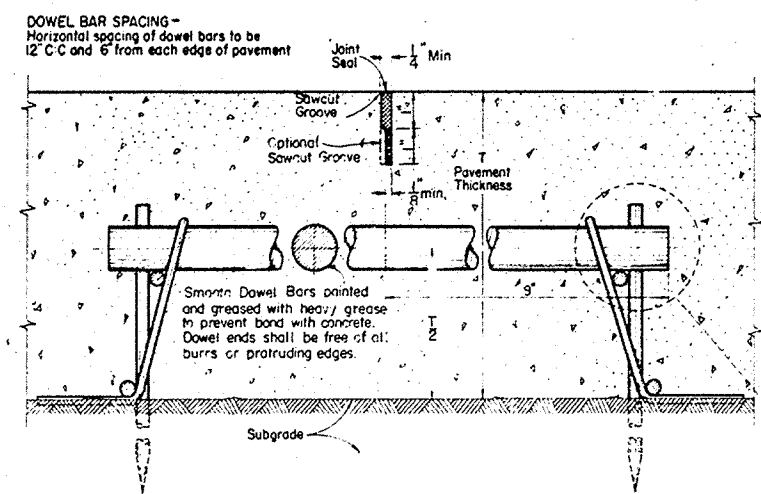
2/6/63
DATE

E. C. Reilly
STATE HIGHWAY ENGINEER

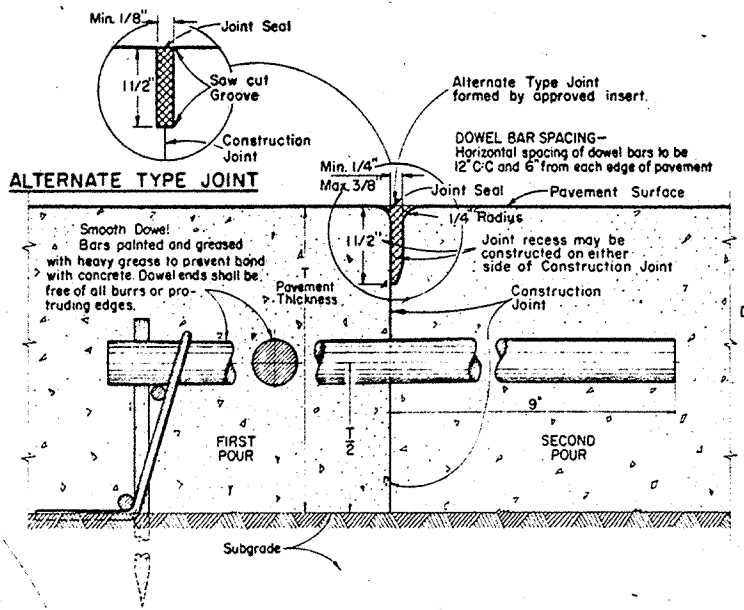
PLATE NO. 4-4.4.6

17-33

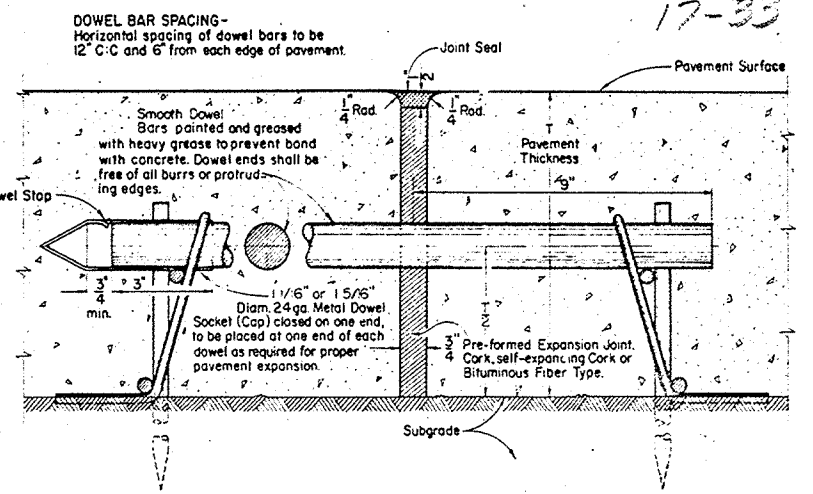
DOWEL BAR DIMENSIONS-
 For 8" P.C PAV'T = 1" Ø X 18"
 " 9" " " = 1 1/4" Ø X 18"
 " 10" " " = 1 1/4" Ø X 18"



CONTRACTION JOINT



CONSTRUCTION JOINT

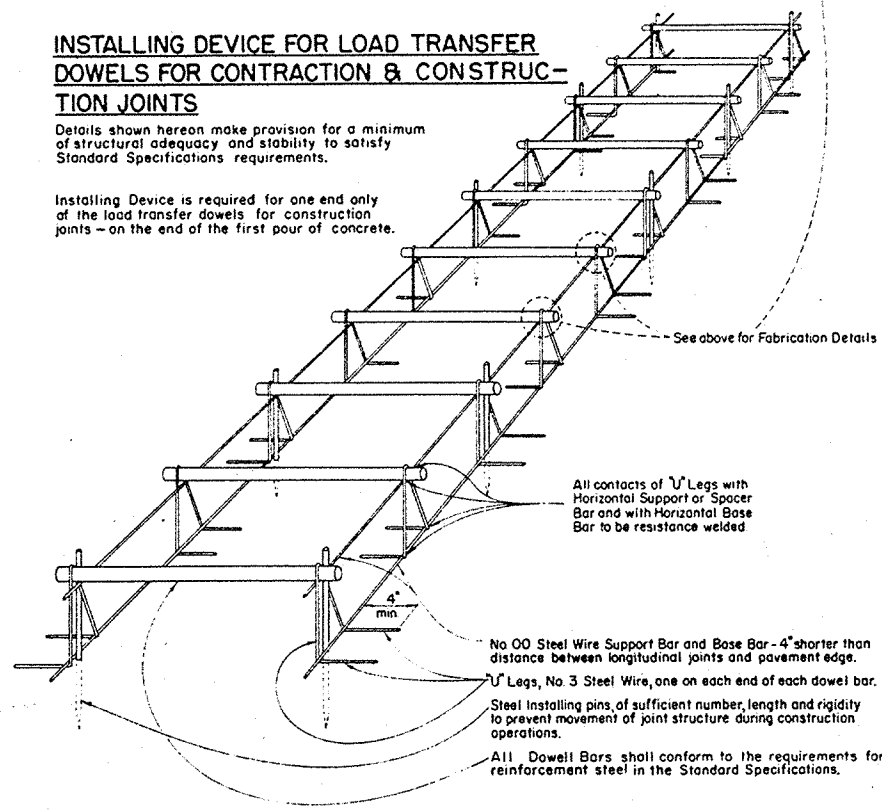


EXPANSION JOINT

INSTALLING DEVICE FOR LOAD TRANSFER DOWELS FOR CONTRACTION & CONSTRUCTION JOINTS

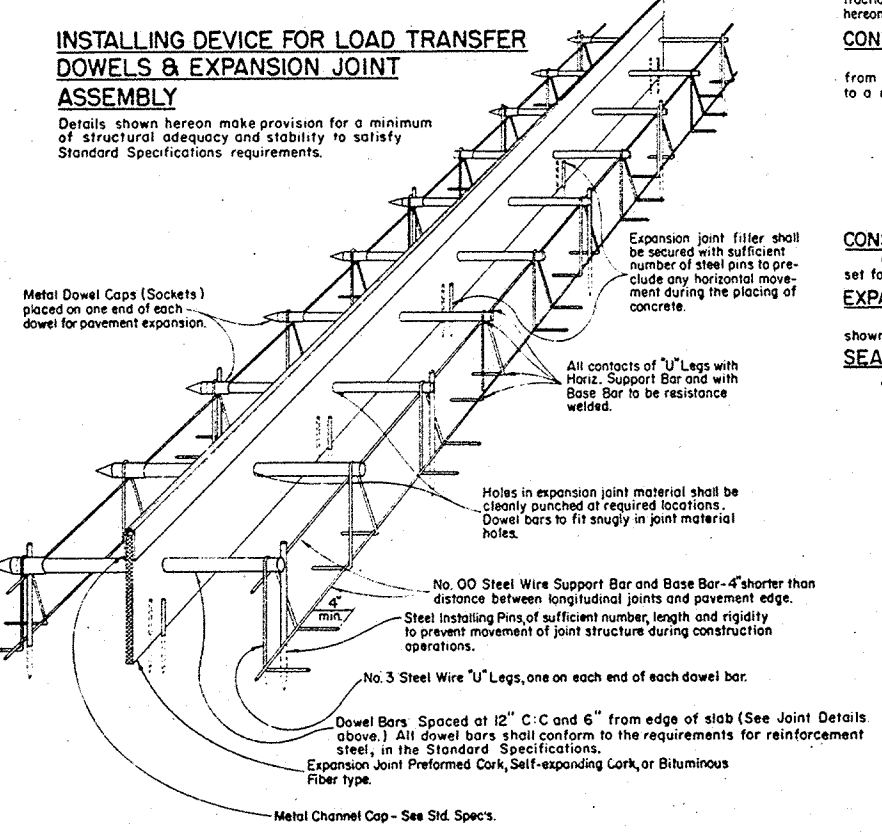
Details shown hereon make provision for a minimum of structural adequacy and stability to satisfy Standard Specifications requirements.

Installing Device is required for one end only of the load transfer dowels for construction joints - on the end of the first pour of concrete.



INSTALLING DEVICE FOR LOAD TRANSFER DOWELS & EXPANSION JOINT ASSEMBLY

Details shown hereon make provision for a minimum of structural adequacy and stability to satisfy Standard Specifications requirements.



GENERAL NOTES-

Details of construction not shown on this drawing shall conform to the pertinent requirements of the Standard Specifications and the applicable Special Provisions.

ALTERNATE DESIGNS-

Alternate designs or methods for installing load transfer dowels for Contraction, Construction and Expansion Joints and appurtenances other than shown hereon may be used upon written approval of the Engineer.

CONTRACTION JOINTS-

Contraction Joints shall be installed at 80' (±2') spacing from adjacent transverse joints, except that lesser spacings ranging to a minimum of 40' shall be used:

- 1) at locations or spacing indicated on the plans.
- 2) as extensions of transverse joints or cracks in abutting pavement lanes.
- 3) at locations designated by the Engineer where there are manholes or other fixtures in the pavement.

CONSTRUCTION JOINTS-

Construction joints shall be installed as necessary, within the limitation set forth in the Standard Specifications.

EXPANSION JOINTS-

Expansion joints are required only at structure approaches and/or where shown on the plans.

SEALING JOINTS-

Joints to be sealed as shown.

**TRANSVERSE JOINTS
 CONCRETE PAVEMENT**

STATE HIGHWAY COMMISSION OF WISCONSIN

RECOMMENDED FOR APPROVAL:

2-5-63
 DATE

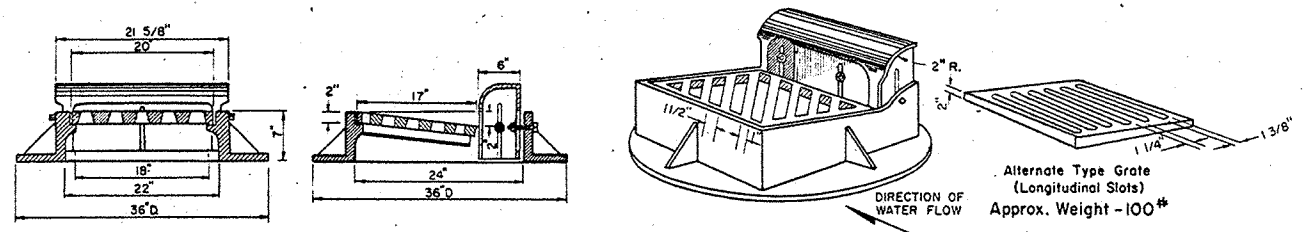
J. D. Pitt
 ENGINEER OF DESIGN

APPROVED:

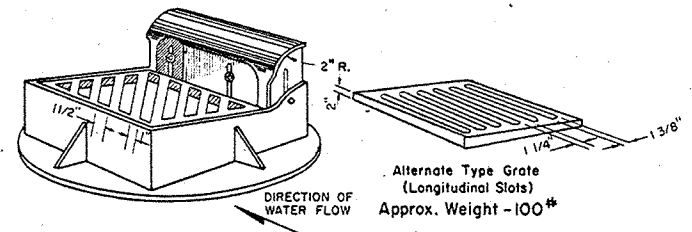
2/6/63
 DATE

E. C. Rostetter
 STATE HIGHWAY ENGINEER

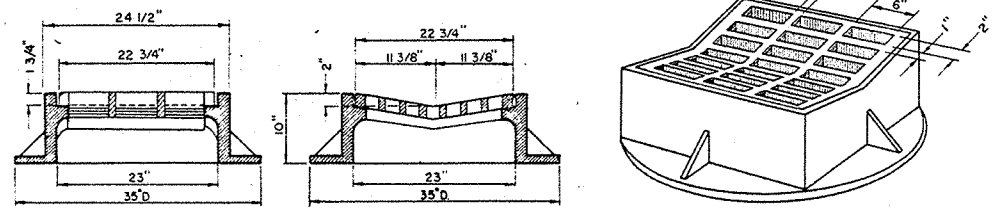
PLATE NO. 4-4.5.9



TYPE "A" - (Approx. Weight 390 Lbs.)
 Frame Weight - 250#
 Grate " - 90#
 Box " - 50#



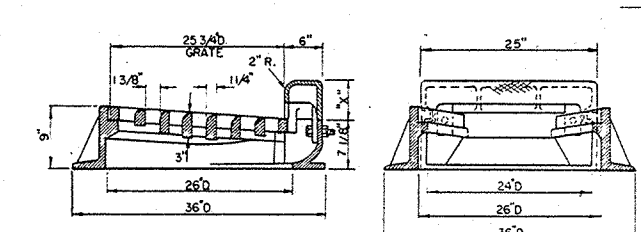
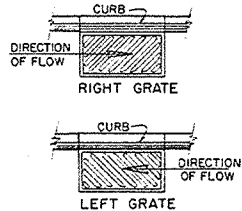
Alternate Type Grate
 (Longitudinal Slots)
 Approx. Weight - 100#



TYPE "B" - (Approx. Weight 414 Lbs.)
 Frame Weight - 275#
 Grate " - 139#

SPECIAL NOTE
 Diagonal slots shall be oriented to the direction of flow as shown hereon. Hence RIGHT and LEFT grates shall be furnished depending on direction of flow. (See Sketch Below)

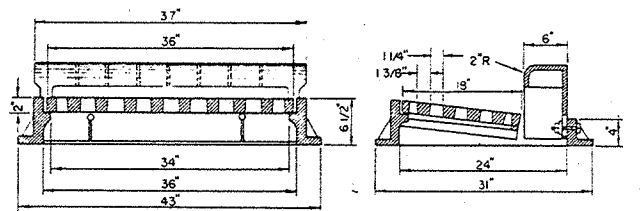
Longitudinal slot type grates may be used ONLY where bicycles are prohibited.



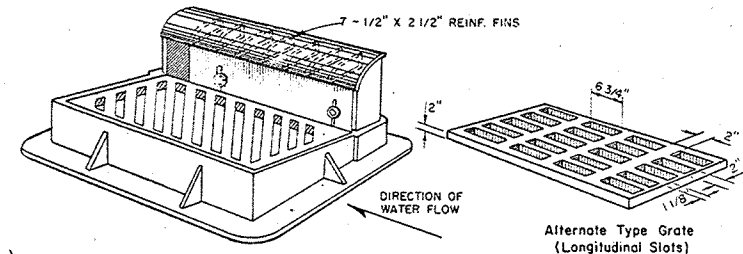
TYPE "G" - (Approx. Weight 425-465 Lbs.)
 Frame Weight - 235#
 Grate " - 130#
 Box - See Table

18-33

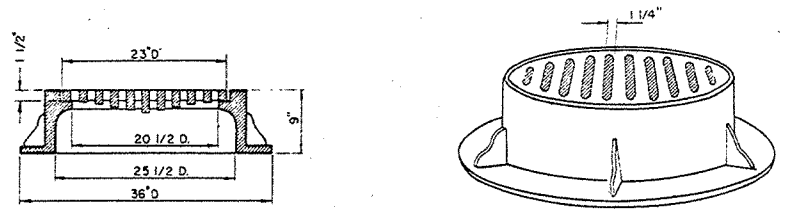
Curb Height	(X)	Weight
4"	4 1/4"	60#
6"	6 1/4"	70#
8"	8 1/4"	90#
10"	10 1/4"	100#



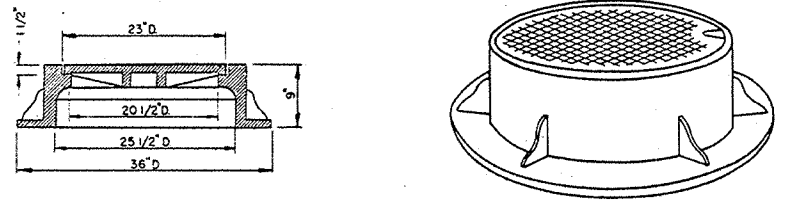
TYPE "H" - (Approx. Weight 530 Lbs.)
 Frame Weight - 220#
 Grate " - 200#
 Box " - 110#



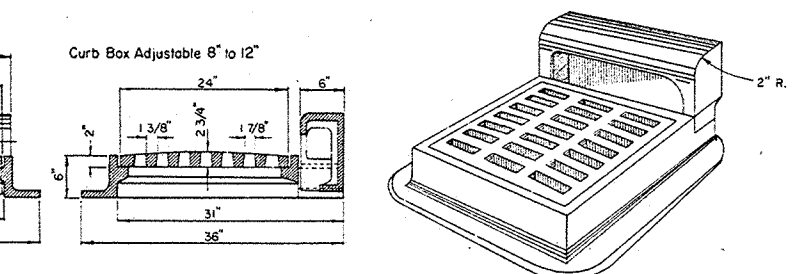
Alternate Type Grate
 (Longitudinal Slots)
 Approx. Weight - 200#



TYPE "C" - (Approx. Weight 370 Lbs.)
 Frame Weight Type "C" & Type "D" - 255#
 Slotted Grate Weight - 115#
 Solid Cover Weight - 150#
 (Note: Frame for Type "C" same as for Type "D")



TYPE "D" - (Approx. Weight 405 Lbs.)
 (Note: Frame for Type "D" same as for Type "C")



TYPE "MI" - (Approx. Weight 575 Lbs.)
 Frame Weight - 275#
 Grate " - 175#
 Box " - 125#

GENERAL NOTES:
 Details of Construction, Materials and Workmanship not shown on this drawing shall conform to the pertinent requirements of the Standard Specifications and the applicable Special Provisions.

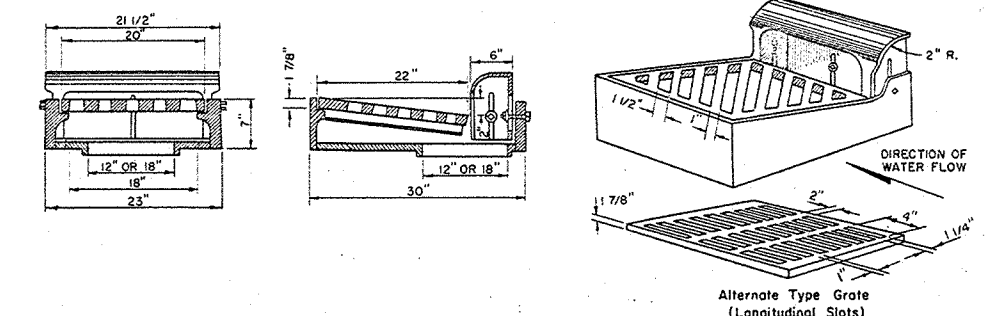
MATERIALS -
 All Iron Castings shown on this drawing shall conform to the requirements for Class 30 of the Standard Specifications for Gray Iron Castings, A.S.T.M. Designation A 48, and the Standard Specifications.

BEARING SURFACES -
 All Catch Basin and Inlet frames and grates which are placed in vehicular traffic areas shall be "Non-Rocking" type, or shall be "Bearing Surface" seated so as to prevent any or all cover noise under traffic.

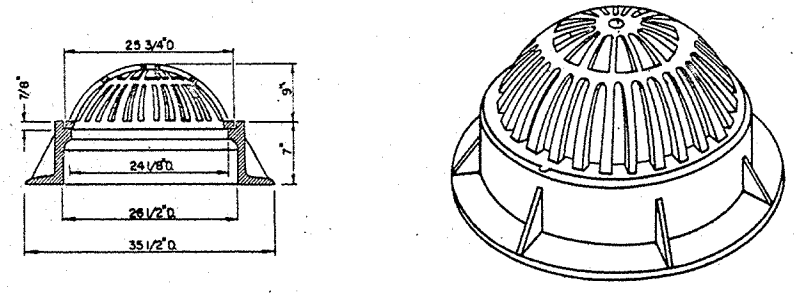
NOMENCLATURE -
 All Catch Basin and Inlet units are designated on the Plans as "Catch Basins 1-A", "2-B" etc., or "Inlets 1-A", "2-B" etc. This designation is interpreted to mean that the number or first digit designates the Masonry portion of the structure, and the following letter or second digit designates the type of cover or Iron Casting (shown hereon) to be used therewith to comprise the complete unit "Catch Basin" or "Inlet" in place.

ADJUSTMENTS -
 Curb Box height to be adjustable 4"-9" unless otherwise noted. Curb Box height to be adjusted after curb form is in place.

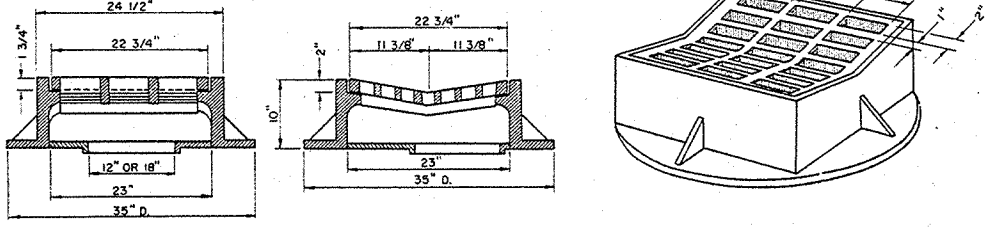
ALTERNATE DESIGNS -
 Detailed drawings for proposed Alternate Designs for "Catch Basin" or "Inlet" Covers may be submitted to the Engineer for approval providing that such Alternate Designs make provision for equivalent capacity and strength.



TYPE "R" - (Approx. Weight 450 Lbs.)



TYPE "E" - (Approx. Weight 325 Lbs.)



TYPE "S" - (Approx. Weight 450 Lbs.)

CATCH BASIN & INLET COVERS

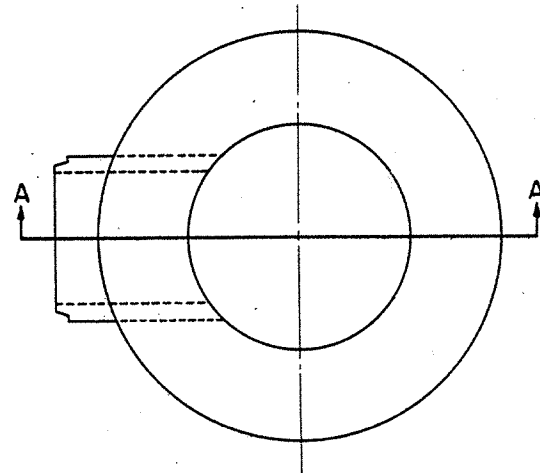
STATE HIGHWAY COMMISSION OF WISCONSIN

RECOMMENDED FOR APPROVAL:

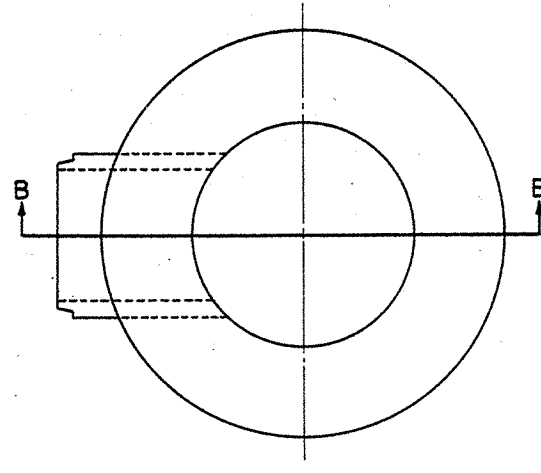
DATE 2-5-63 *J. P. Pitt*
 APPROVED: ENGINEER OF DESIGN

DATE 2/6/63 *E. S. Redden*
 APPROVED: STATE HIGHWAY ENGINEER

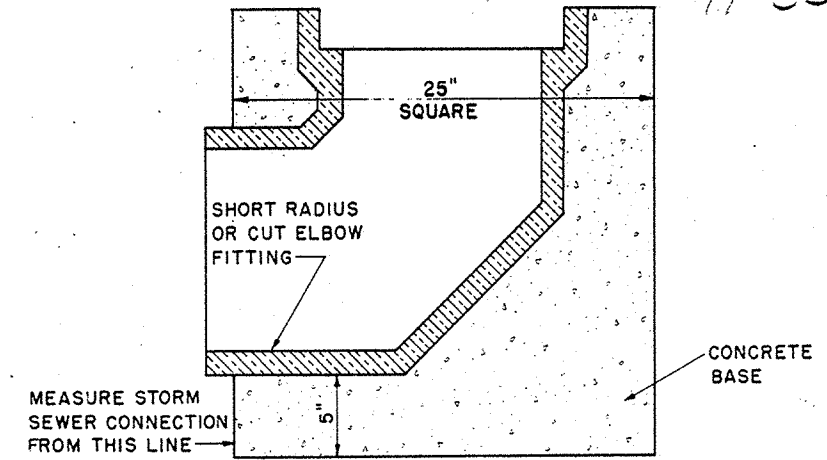
PLATE NO. 5-3.4.6



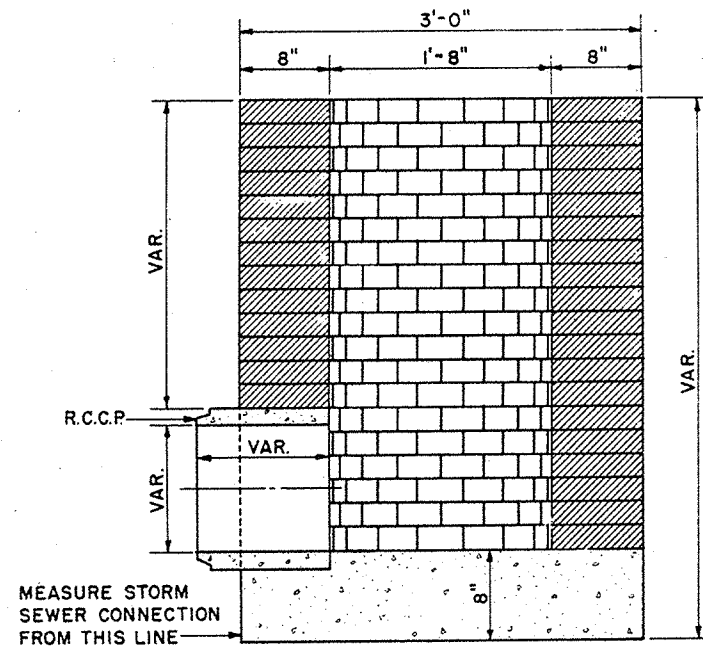
TOP OF INLET



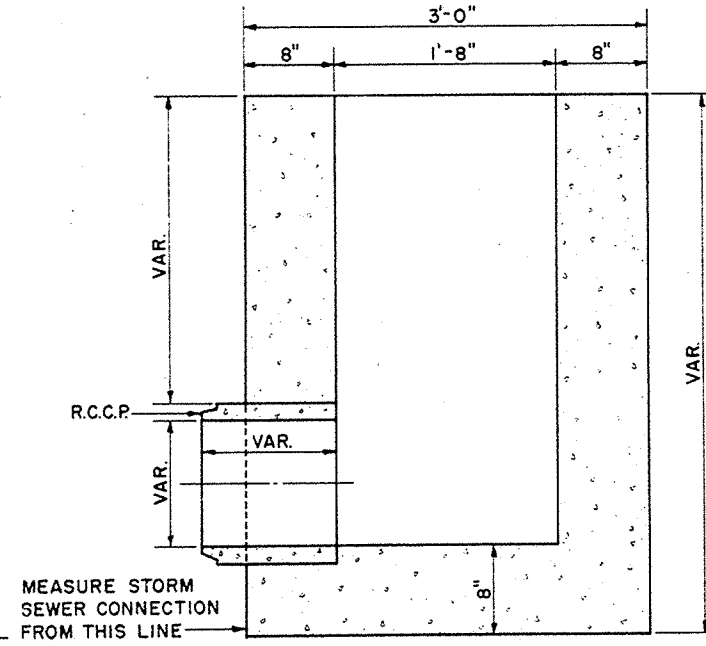
TOP OF INLET



TYPE 2



SECTION A-A
BRICK



SECTION B-B
CONCRETE

TYPE I

GENERAL NOTES

DETAILS RELATIVE TO THESE ITEMS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS. VARIATIONS IN DIMENSIONS AND DESIGN WILL BE PERMISSIBLE PROVIDING EQUIVALENT CAPACITY AND STRENGTH ARE ATTAINED. ARRANGEMENT, SIZE AND NUMBER OF INLET AND DISCHARGE PIPES SHALL CONFORM TO THE NEEDS OF THE PERTINENT LOCATION. INLETS ARE CALLED FOR ON THE PLANS AS "INLETS 1-N," "INLETS 2-R," ETC., THE NUMERAL DESIGNATES THE MASONRY PORTION OF THE STRUCTURE AND THE LETTER DESIGNATES THE COVER TO BE USED THEREON. WHEN CONCRETE IS USED IN THE CONSTRUCTION OF TYPE 1 INLET, THE TOP OF THE MASONRY SHALL BE LEFT SUFFICIENTLY LOW TO PERMIT PROPER ADJUSTMENT OF COVER TO GRADE BY THE USE OF MORTAR AND BRICK.

INLETS

STATE HIGHWAY COMMISSION OF WISCONSIN

RECOMMENDED FOR APPROVAL:

DATE 7/27/53 *[Signature]*
CONSTRUCTION ENGINEER

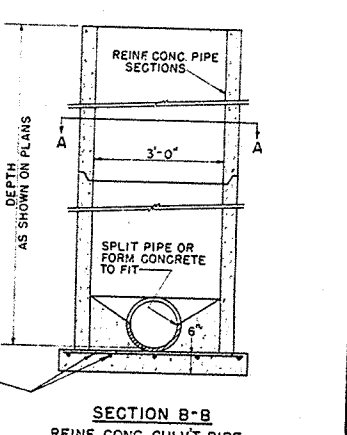
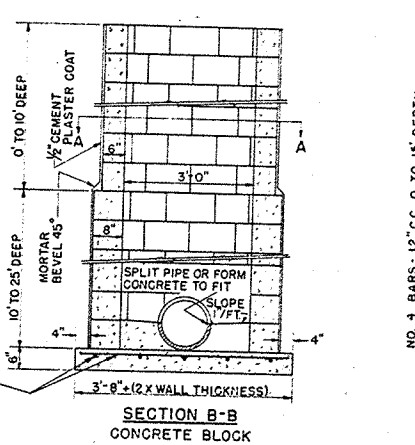
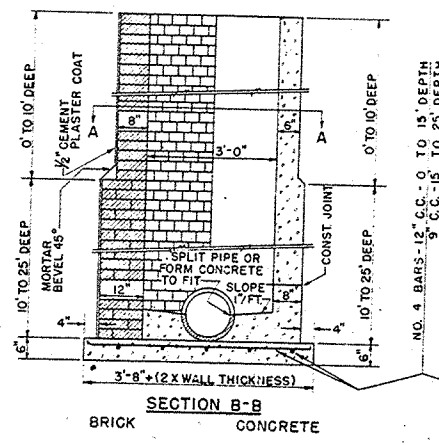
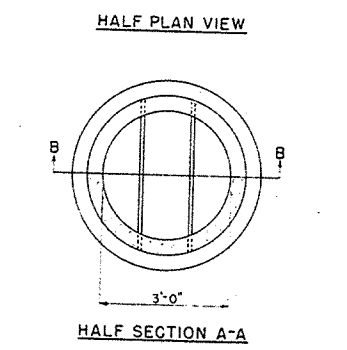
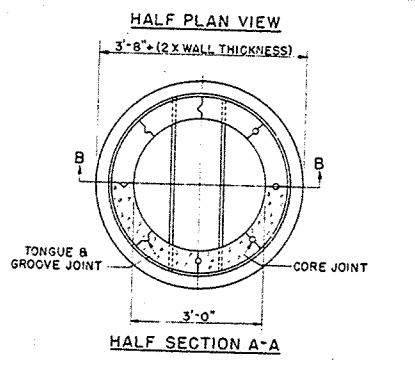
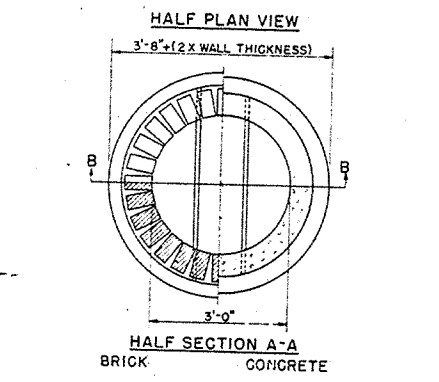
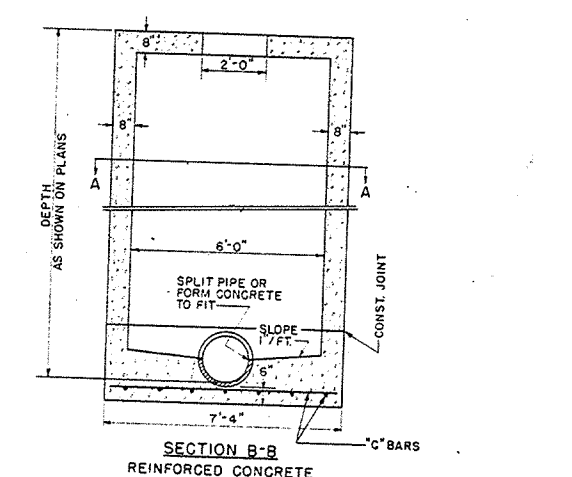
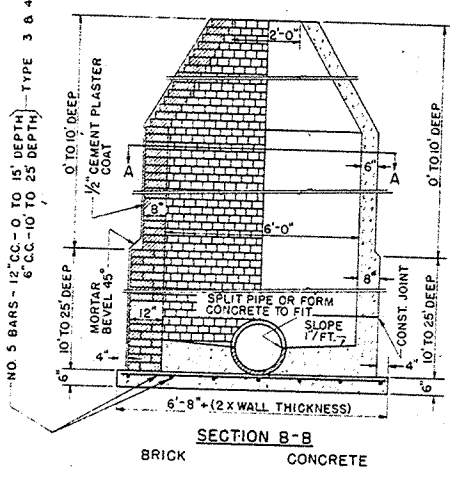
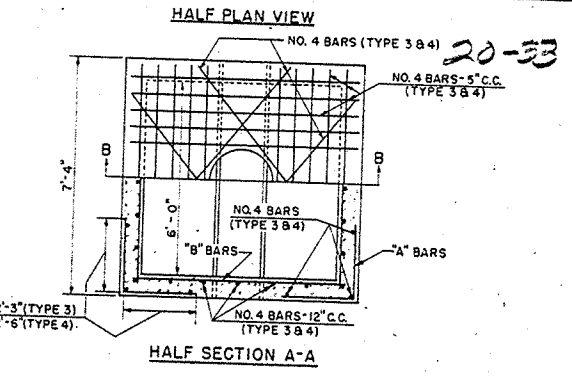
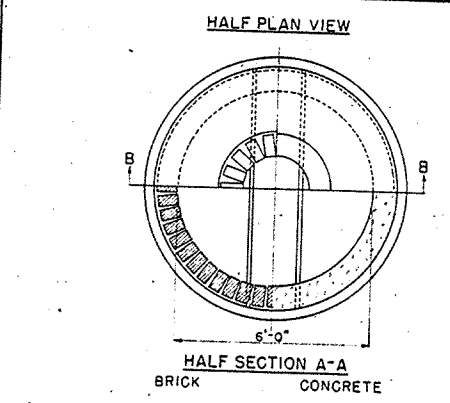
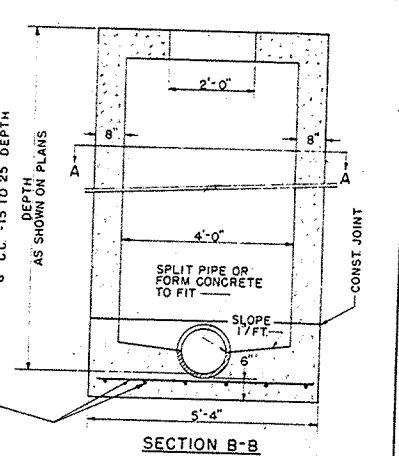
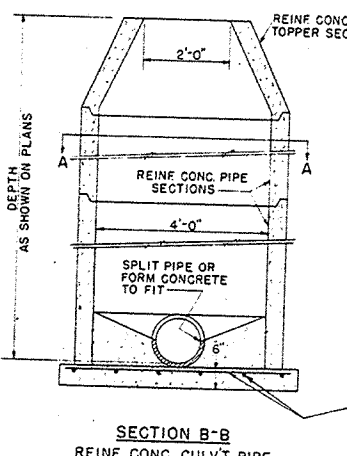
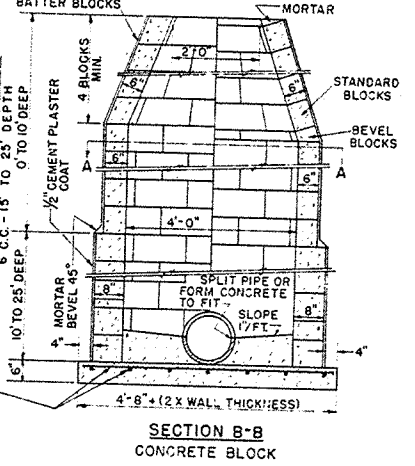
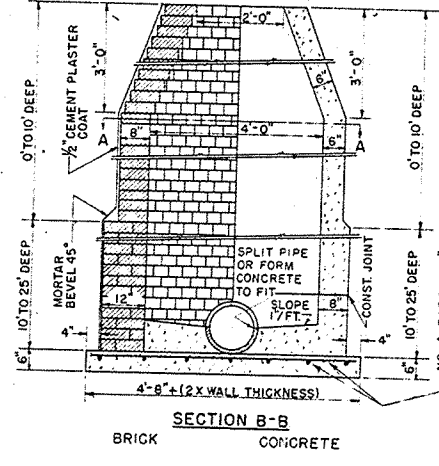
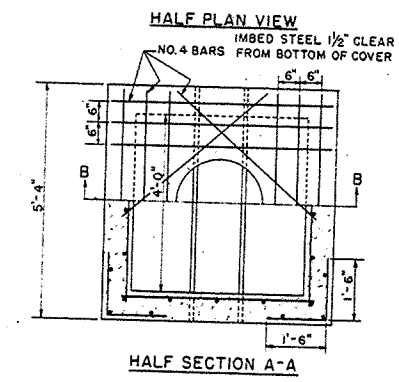
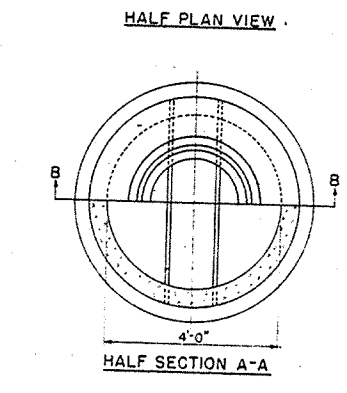
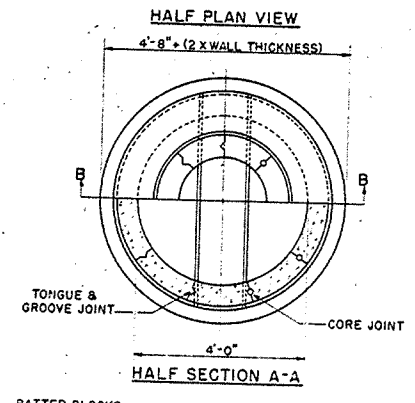
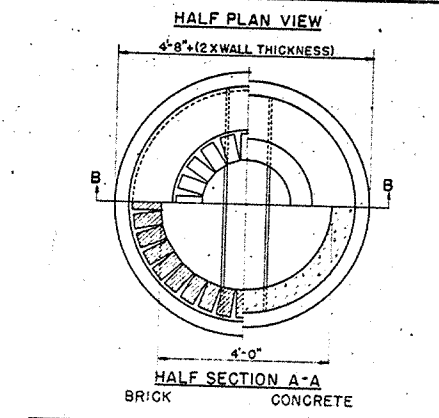
APPROVED:

DATE 1/27/53 *[Signature]*
STATE HIGHWAY ENGINEER

DRAWN LJD
CHECKED

5-3.5.1

5-3.5.1



CONSTRUCTION NOTES
DETAILS RELATIVE TO THESE ITEMS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS. STEPS SHALL BE INSTALLED IN ALL MANHOLES.

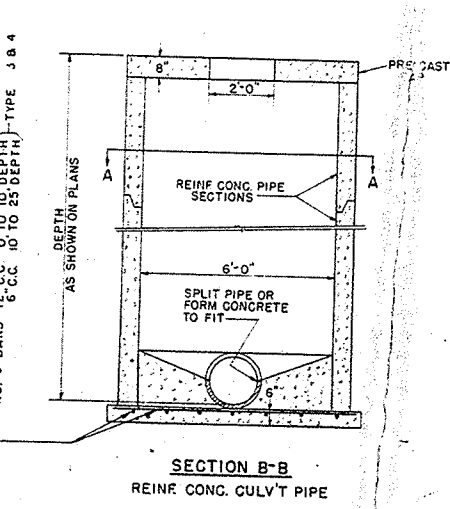
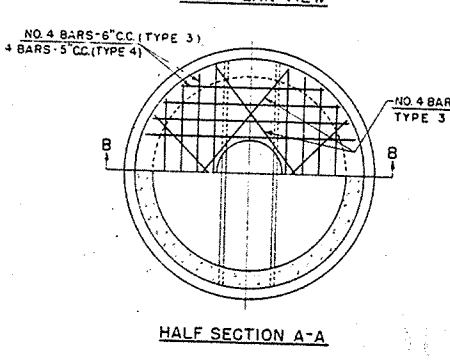
VARIAIONS IN DIMENSIONS AND DESIGN WILL BE PERMISSABLE PROVIDING EQUIVALENT CAPACITY AND STRENGTH ARE ATTAINED.

STRUCTURES ARE CALLED FOR ON THE PLANS AS "MANHOLES 1'-J", "MANHOLES 2'-K" ETC. THE NUMERICAL DESIGNATES THE OPTIONAL MASONRY PORTION OF THE STRUCTURE AND THE LETTER DESIGNATES THE COVER TO BE USED THEREON.

THE REINFORCED CONCRETE TOPPER SECTION AS SHOWN FOR TYPE 1, REINF. CONC. CULVT PIPE MANHOLE, MAY BE USED AS AN ALTERNATE CONE ON TYPE 1 BRICK, CONCRETE OR CONCRETE BLOCK MANHOLES.

WHEN ANY STRUCTURE IS CONSTRUCTED OF CONCRETE, CONCRETE BLOCK OR REINF. CONC. CULVT PIPE, THE TOP OF THE MASONRY SHALL BE LEFT SUFFICIENTLY LOW TO PERMIT PROPER ADJUSTMENT OF COVER TO GRADE BY THE USE OF MORTAR AND BRICK.

DESIGN NOTES
INSIDE DIMENSIONS FOR MANHOLES:
USE 3' DIAMETER WHEN NECESSARY TO PLACE IN CONSTRUCTED AREAS.
USE 4' DIAMETER WHEN SEWER IS UNDER 18" IN DIAMETER.
USE 6' DIAMETER OR 6' SQUARE WHEN SEWER IS 18" TO 54" DIAMETER.



TYPE 3

"A" BARS - NO. 4 BARS - 12" C.C. - 0 TO 10' DEPTH
6" C.C. - 10' TO 25' DEPTH

"B" BARS - NO. 4 BARS - 12" C.C. - 0 TO 25' DEPTH
10" C.C. - 10' TO 25' DEPTH

"C" BARS - NO. 5 BARS - 12" C.C. - 0 TO 10' DEPTH
6" C.C. - 10' TO 25' DEPTH

TYPE 4

FOR SEWER GREATER THAN 54" DIAMETER SPECIAL DESIGN IS REQUIRED SHOWING INSIDE DIMENSION OF MANHOLE TO BE DIAMETER OF SEWER PLUS 18". SPECIAL DESIGN SHALL CONFORM TO THE GENERAL DETAILS FOR TYPE 3.

"A" BARS - NO. 5 BARS - 12" C.C. - 0 TO 10' DEPTH.
6" C.C. - 10' TO 25' DEPTH.

"B" BARS - NO. 4 BARS - 12" C.C. - 0 TO 10' DEPTH.
10" C.C. - 10' TO 25' DEPTH.

"C" BARS - NO. 5 BARS - 10" C.C. - 0 TO 10' DEPTH.
5" C.C. - 10' TO 25' DEPTH.

MANHOLES

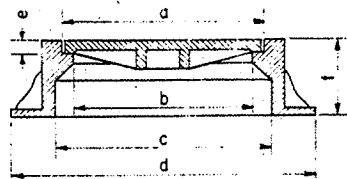
STATE HIGHWAY COMMISSION OF WISCONSIN

RECOMMENDED FOR APPROVAL

DATE 2-5-63

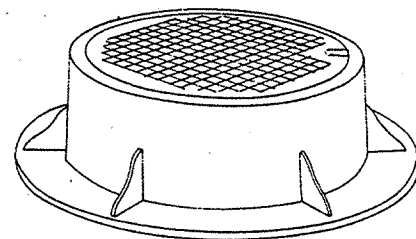
APPROVED: [Signature] ENGINEER OF DESIGN

DATE 2/16/63 [Signature] STATE HIGHWAY ENGINEER

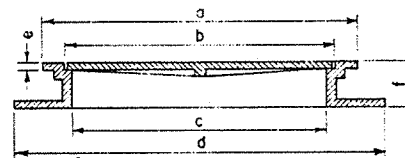


	a	b	c	d	e	f	Weight
TYPE "J"	23"	20 1/2"	25 1/2"	36"	1 1/2"	9"	100 Lbs.
TYPE "K"	32 1/2"	30"	36"	48"	1 1/2"	10"	750 Lbs.

TYPE "J" & "K"

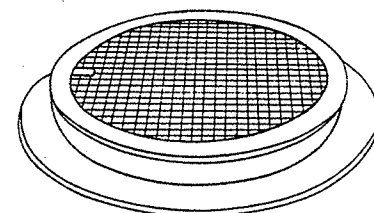


21-33



	a	b	c	d	e	f	Weight
TYPE "L"	25"	24"	22 1/2"	33"	5/8"	4"	210 Lbs.
TYPE "M"	42"	37 1/2"	36"	47"	3/4"	4"	475 Lbs.

TYPE "L" & "M"



CONSTRUCTION NOTES

DETAILS RELATIVE TO THESE ITEMS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

VARIATIONS IN DIMENSIONS AND DESIGN WILL BE PERMISSABLE PROVIDING EQUIVALENT CAPACITY AND STRENGTH ARE ATTAINED.

MANHOLES ARE CALLED FOR ON THE PLANS AS "MANHOLES 1-J", "MANHOLES 2-K" ETC., THE NUMERAL DESIGNATES THE MASONRY PORTION OF THE STRUCTURE AND THE LETTER DESIGNATES THE FIXTURE TO BE USED THEREON. MANHOLE FRAMES AND COVERS SUBJECT TO TRAFFIC TO BE MACHINED OR NON-ROCKING TYPE.

DESIGN NOTES

TYPES "J" & "K" TO BE USED WHERE FACILITY WILL BE SUBJECT TO TRAFFIC.

TYPES "L" & "M" TO BE USED WHERE FACILITY WILL NOT BE SUBJECT TO TRAFFIC.

MANHOLE COVERS

STATE HIGHWAY COMMISSION OF WISCONSIN

RECOMMENDED FOR APPROVAL:

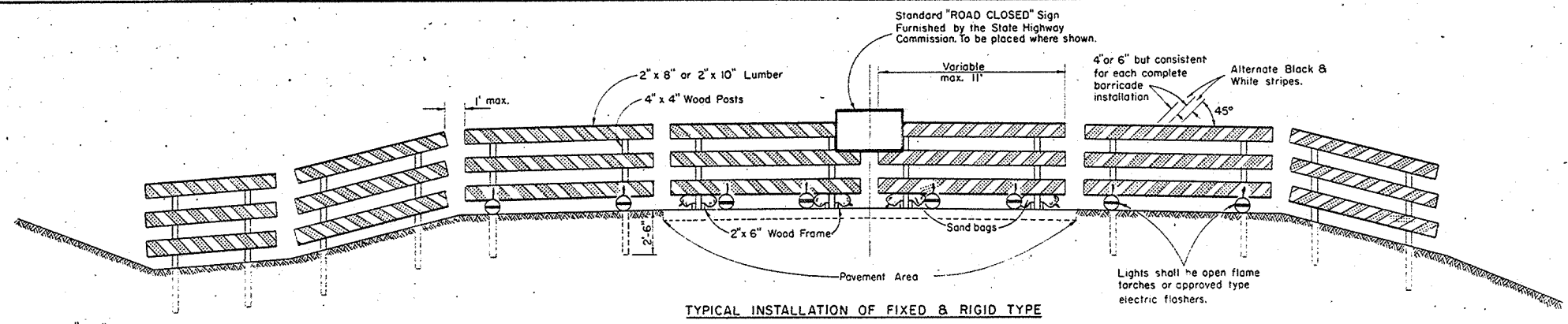
DATE 1/27/53 [Signature]
CONSTRUCTION ENGINEER

APPROVED:

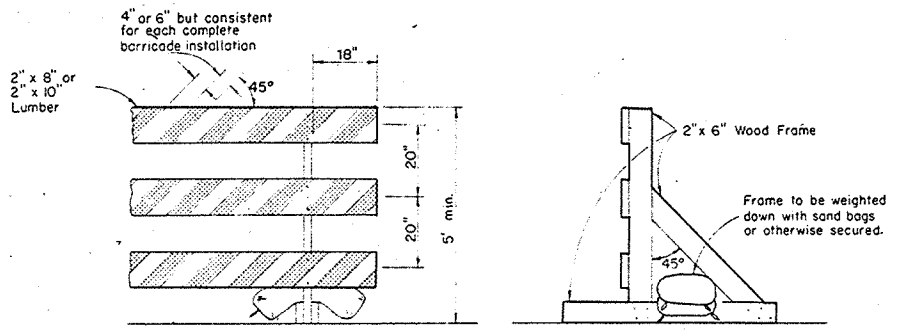
DATE 1/27/53 [Signature]
STATE HIGHWAY ENGINEER

DRAWN LJD
CHECKED

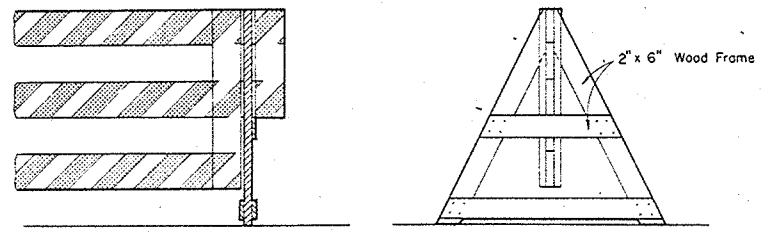
5-3.8.2



TYPICAL INSTALLATION OF FIXED & RIGID TYPE

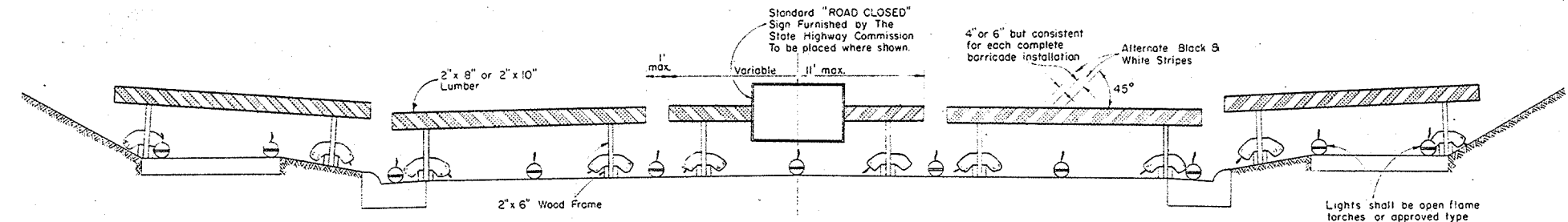


ALTERNATE TYPE INSTALLATION (RIGID)

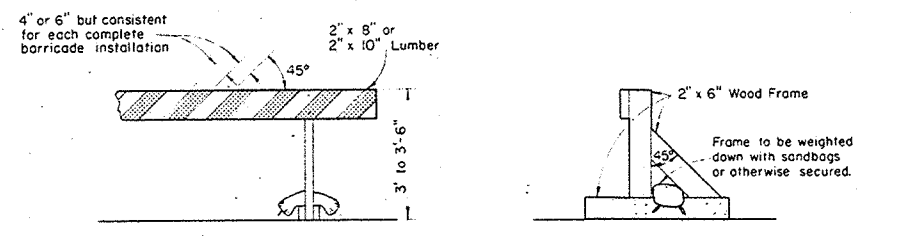


ALTERNATE TYPE INSTALLATION (DEMOUNTABLE)

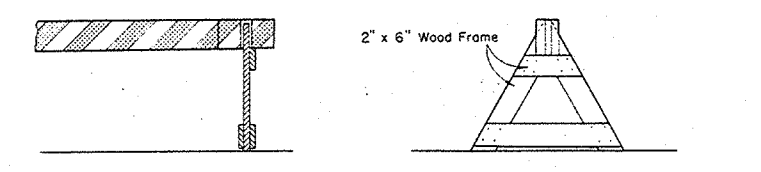
CLASS I BARRICADE



TYPICAL INSTALLATION OF RIGID TYPE



ALTERNATE TYPE INSTALLATION (RIGID)



ALTERNATE TYPE INSTALLATION (DEMOUNTABLE)

CLASS II BARRICADE

22-33

GENERAL NOTES:

The Contractor shall construct, place and maintain barricades as shown on this drawing and as required by the Standard Specifications for the duration of the project at all points of highway closure. Barricades shall be painted as shown hereon and structurally maintained for maximum visibility at all times, for the duration of the respective project.

CLASS I BARRICADE

Shall be used at points of closure where road is closed to traffic. Gates or movable sections of barricade shall be provided when necessary, for access of equipment or other authorized vehicles only.

CLASS II BARRICADE

May be used only where the hazard to traffic is relatively small, and for the more or less continuous delimiting of a restricted roadway, or for temporary daytime use.

LUMBER & FABRICATION

Lumber shall be of a grade structurally sound and sufficiently rigid to satisfactorily support and maintain the purpose and intent of a barricade facility. The fabrication of the barricade shall be in accord with good pertinent wood-working practices.

PAINTING

Barricades shall be painted as shown hereon in alternate black and white stripes. Black stripes shall be painted with weather resistant and durable black paint. White stripes shall be painted a prime coat of good grade wood primer, followed by two coats of white "Codic Reflective Liquid" (Minnesota Mining Co.) or equivalent, or reflective sheeting wide angle, flat top "Scotchlite" brand material (Minnesota Mining Co.) or equivalent.

DIRECTION OF DIAGONAL STRIPES

Where a barricade extends entirely across the roadway and no vehicle access provision, the stripes shall slope downward toward the highway centerline. Where vehicle access is permitted, the stripes shall slope downward in the direction toward which vehicles must turn in detouring. Where both right and left turns are provided for, the stripes shall slope downward in both directions from the center.

MEASUREMENT & PAYMENT

All barricades, unless otherwise provided for in the plans and/or special provisions shall be furnished, placed, and maintained as noted above, and no additional compensation will be allowed but shall be construed to be included in the price bid for other items.

NOTE:

Lighting devices for barricades shall conform to the requirements of the Standard Specifications.

NOTE:

All lumber or timber dimensions shown hereon are nominal.

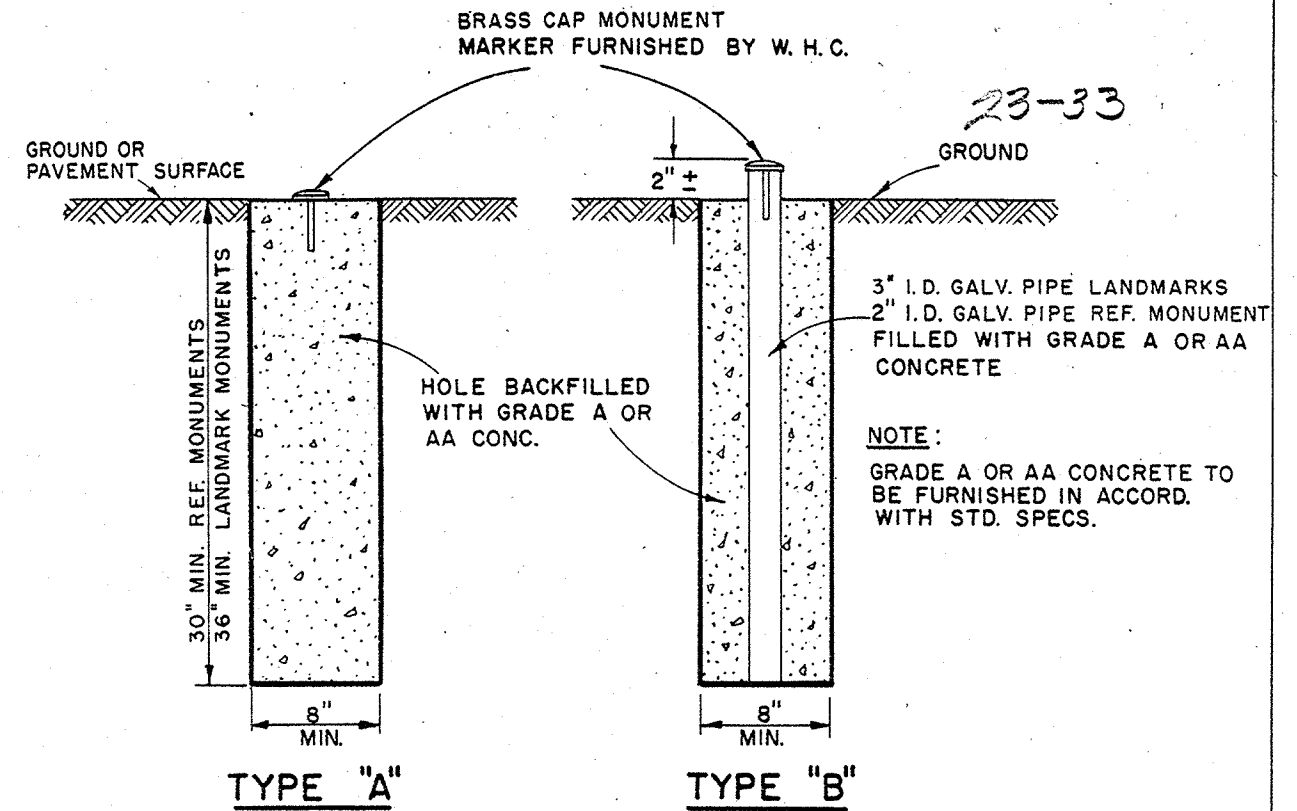
CONSTRUCTION BARRICADE	
STATE HIGHWAY COMMISSION OF WISCONSIN	
RECOMMENDED FOR APPROVAL:	
DATE: 3-5-63	<i>J. P. Pitt</i> ENGINEER OF DESIGN
APPROVED:	
DATE: 2/16/63	<i>P. C. Brittain</i> STATE HIGHWAY ENGINEER
PLATE NO. 7-4.1.4	

REF
E 1/4 COR.
SEC + 35
T22N-R6W
69.7'N

T25N R17E
S23 | S24
S26 | S25
1961

REF
E 1/4 COR.
+
S35T22NR6W
89.87'N

**BRASS
MONUMENT MARKER**
(FURNISHED TO CONTRACTOR
WITHOUT COST.)



GENERAL NOTES

MONUMENT CONSTRUCTION SHOWN HEREON CONFORMS TO WISCONSIN STATUTES, SECTIONS 59.635 AND 60.37 BRASS MONUMENT MARKER TO BE FURNISHED AND PROPERLY STAMPED BY WIS. HIGHWAY COMM. UNLESS OTHERWISE SPECIFIED IN THE CONTRACT. MONUMENTS CONFORMING TO EITHER TYPE A OR TYPE B AS SHOWN HEREON TO BE PLACED WHERE AND AS DIRECTED BY THE ENGINEER.

MEASUREMENT AND PAYMENT-

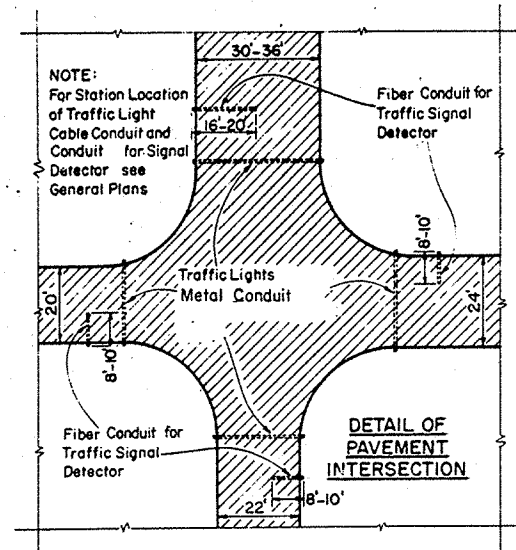
LANDMARK REFERENCE MONUMENTS SHALL BE MEASURED AND PAID FOR AS COMPLETED UNITS IN PLACE, WHICH PRICE SHALL INCLUDE FURNISHING AND PLACING OF CONCRETE, PIPE, EXCAVATION AND DISPOSITION THEREOF, PLACEMENT OF BRASS MARKER AND SITE RESTORATION.

**LANDMARK
REFERENCE MONUMENTS**

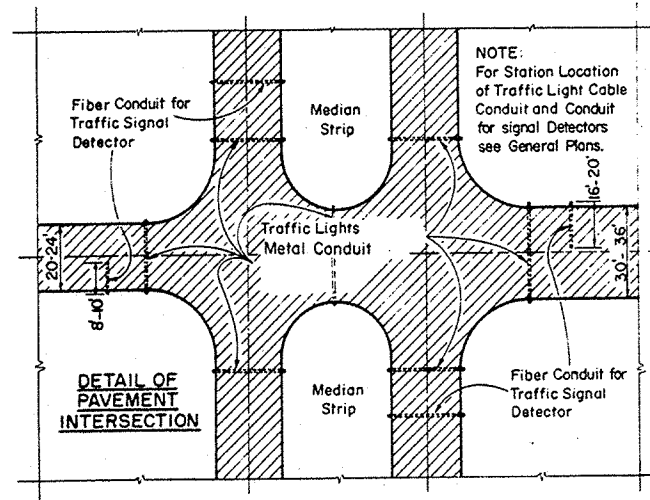
STATE HIGHWAY COMMISSION OF WISCONSIN
RECOMMENDED FOR APPROVAL:

2-5-63
DATE
APPROVED: *J. J. Pelt*
ENGINEER DESIGN

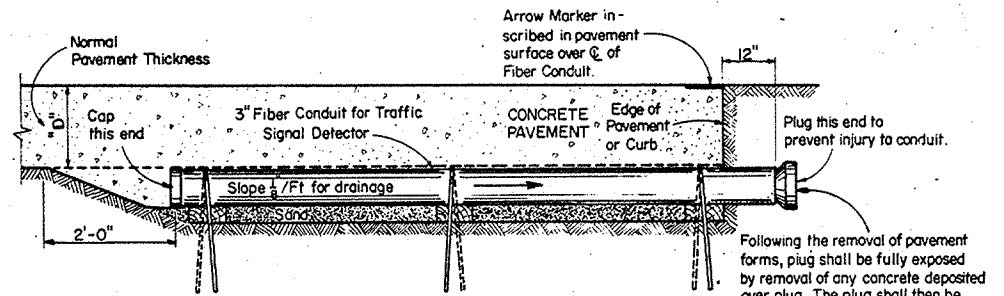
7/6/63
DATE
APPROVED: *E. C. Rostetter*
STATE HIGHWAY ENGINEER 12-1.1.2



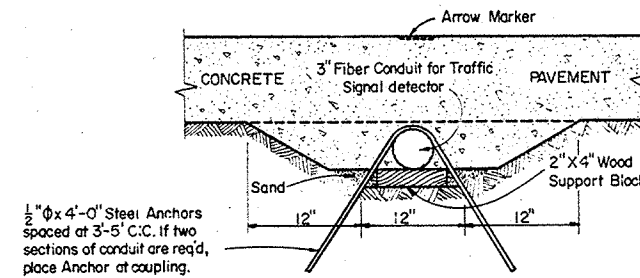
PLAN VIEW
SHOWING RELATIVE POSITION OF
TRAFFIC LIGHT CONDUITS AND
TRAFFIC SIGNAL DETECTOR CONDUITS
AT UNDIVIDED HIGHWAY INTERSECTIONS



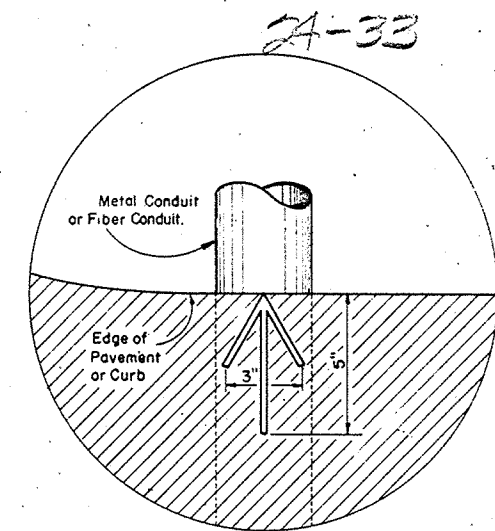
PLAN VIEW
SHOWING RELATIVE POSITION OF
TRAFFIC LIGHT CONDUITS AND
TRAFFIC SIGNAL DETECTOR CONDUITS
AT DIVIDED HIGHWAY INTERSECTIONS



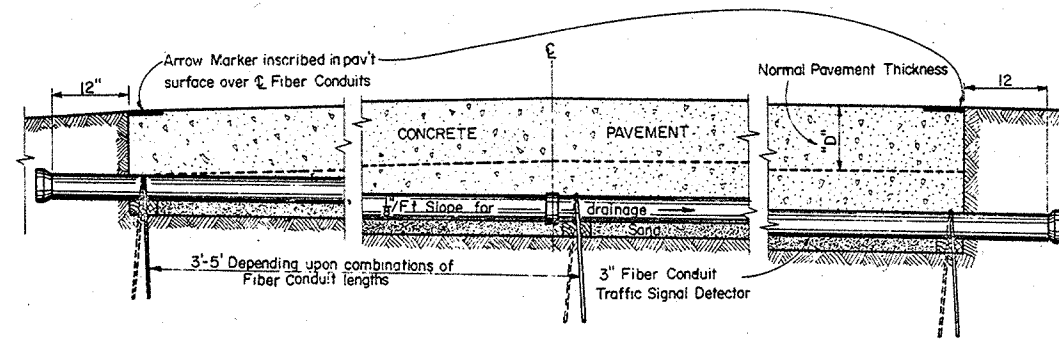
TRAFFIC SIGNAL DETECTOR FOR
UNDIVIDED HIGHWAYS



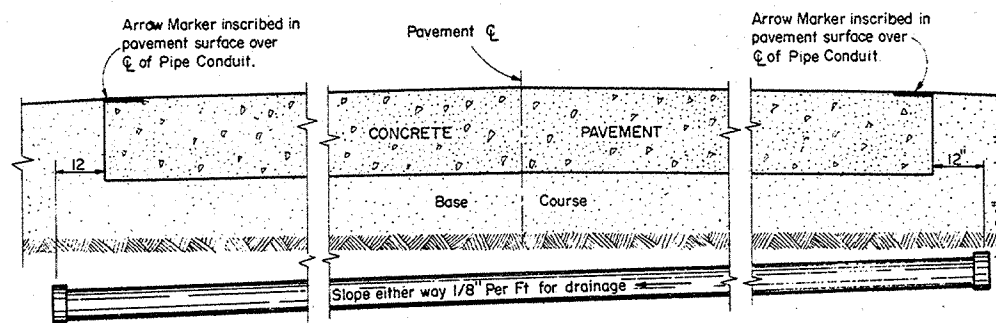
SIDE & END ELEVATIONS
SHOWING PLACEMENT DETAILS
FOR TRAFFIC SIGNAL DETECTOR CONDUIT



PLAN VIEW - ARROW MARKER



TRAFFIC SIGNAL DETECTOR FOR
DIVIDED HIGHWAYS



ELEVATION ON CENTERLINE
SHOWING PLACEMENT DETAILS
FOR TRAFFIC SIGNAL CONDUIT

GENERAL NOTES

Details of Construction not shown on this drawing shall conform to the pertinent requirements of the Standard Specifications, and the applicable Special Provisions.

MATERIALS

Metal Conduit shall be furnished and placed as shown herein and in accord with the Standard Specifications.

Fiber Conduit shall be furnished and placed as shown herein and in accord with the Standard Specifications.

MEASUREMENT & PAYMENT

The item of Fiber Conduit shall be measured and paid for by the linear foot complete in place and in accord with Standard Specifications

CONDUIT SIZES

Unless shown or required otherwise on the plans, Metal Conduit under Roadways shall be 3" I.D. and Metal Conduit under Driveways and Private Entrances shall be 2" I.D.

**METAL CONDUIT
&
FIBER CONDUIT**

STATE HIGHWAY COMMISSION OF WISCONSIN

RECOMMENDED FOR APPROVAL:

2-5-63
DATE

J. J. Pitt
ENGINEER OF DESIGN

APPROVED:

2/6/63
DATE

E. G. Rottiger
STATE HIGHWAY ENGINEER

PLATE NO. 8-5.1.5

S.P.R. DISTRICT OFFICE	PROJECT	SHEET NUMBER	TOTAL SHEETS
WIS. 4	S 0324 (5)	25	33

FINAL SURVEY NOTE BOOK NO. 530
 SURVEYED, PLOTTED, AREA CHECKED
 BY DATE

ORIGINAL SURVEY NOTE BOOK NO. 530
 SURVEYED, PLOTTED, AREA CHECKED
 BY DATE



STATION	DISTANCE	YARDAGE	
		EXCAVATION	FILL
16+750	124		
15+50	87		0
11	176		0
150	189		0
12	189		0
150	174		0
13	163		0
150	165		0
14	150		0
150	141		0
15	144		0
150	146		0
16	157		0
150	170		0
SHEET TOTAL		2,051	0

SCALE: 1" = 5'

FINAL SURVEY
 NO. 390
 AREAS CHECKED

DATE
 10-21-52
 BY
 H. A. G. E. G.



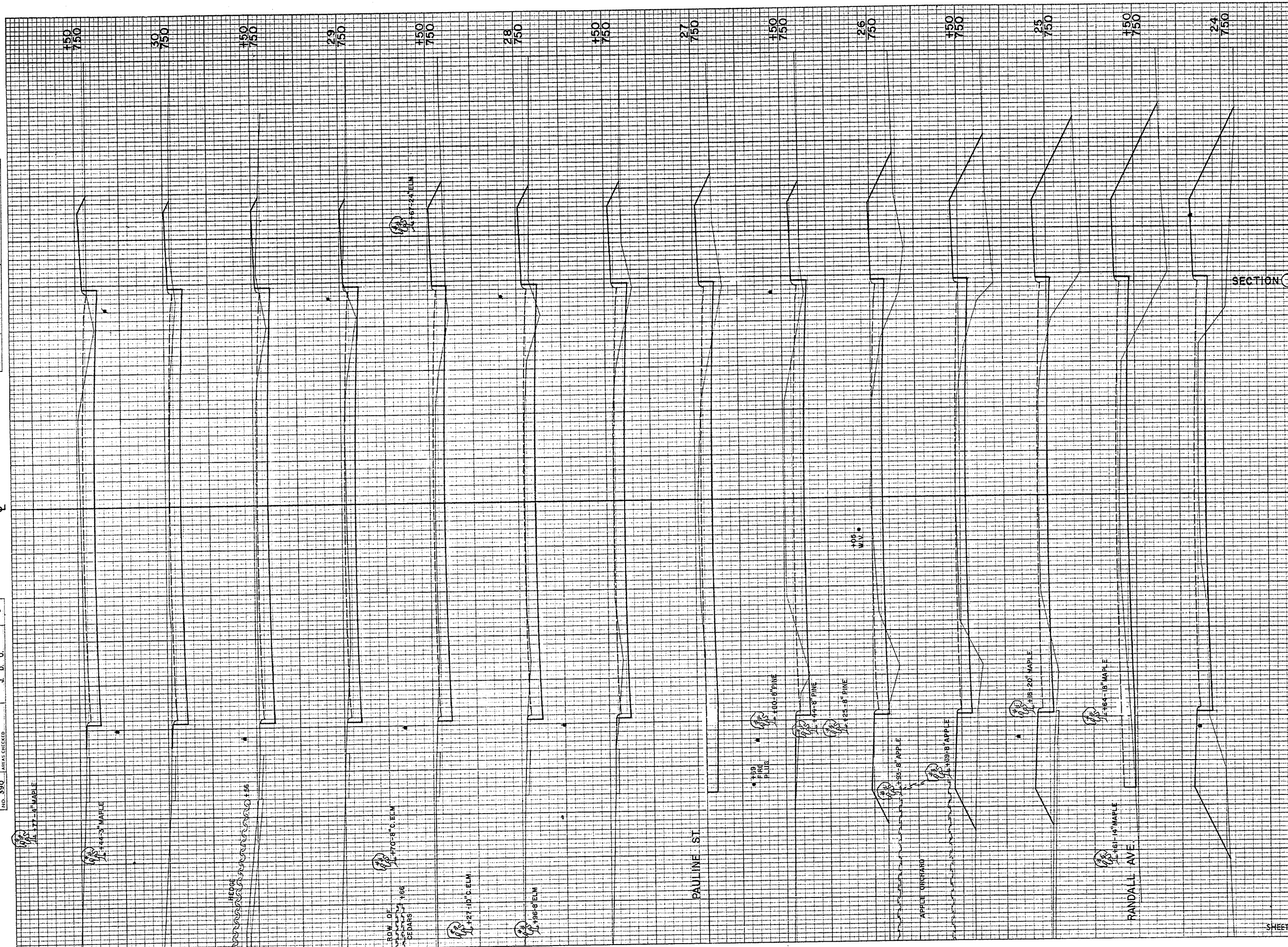
B.P.R. DISTRICT OFFICE	PROJECT	SHEET NUMBER	TOTAL SHEETS
WIS. 4	S 0324 (5)	26	33

STATION	DISTANCE	YARDAGE	
		UNCL.	FILL
16	150		
17	163		0
150	161		0
18	148		0
150	148		0
19	178		0
150	194		0
20	193		0
150	180		2
21	122		20
150	63		46
22	33		102
150	35		152
23	41		185
150	61		135
SHEET TOTAL		1,720	642

BY DATE
 SURVEYED
 PLOTTED
 AREAS
 CHECKED
 FINAL SURVEY NOTE BOOK NO. 390

DATE 10-26-62
 BY D. A. N. A. C. F. H. J. D. G.
 ORIGINAL SURVEY NOTE BOOK NO. 390

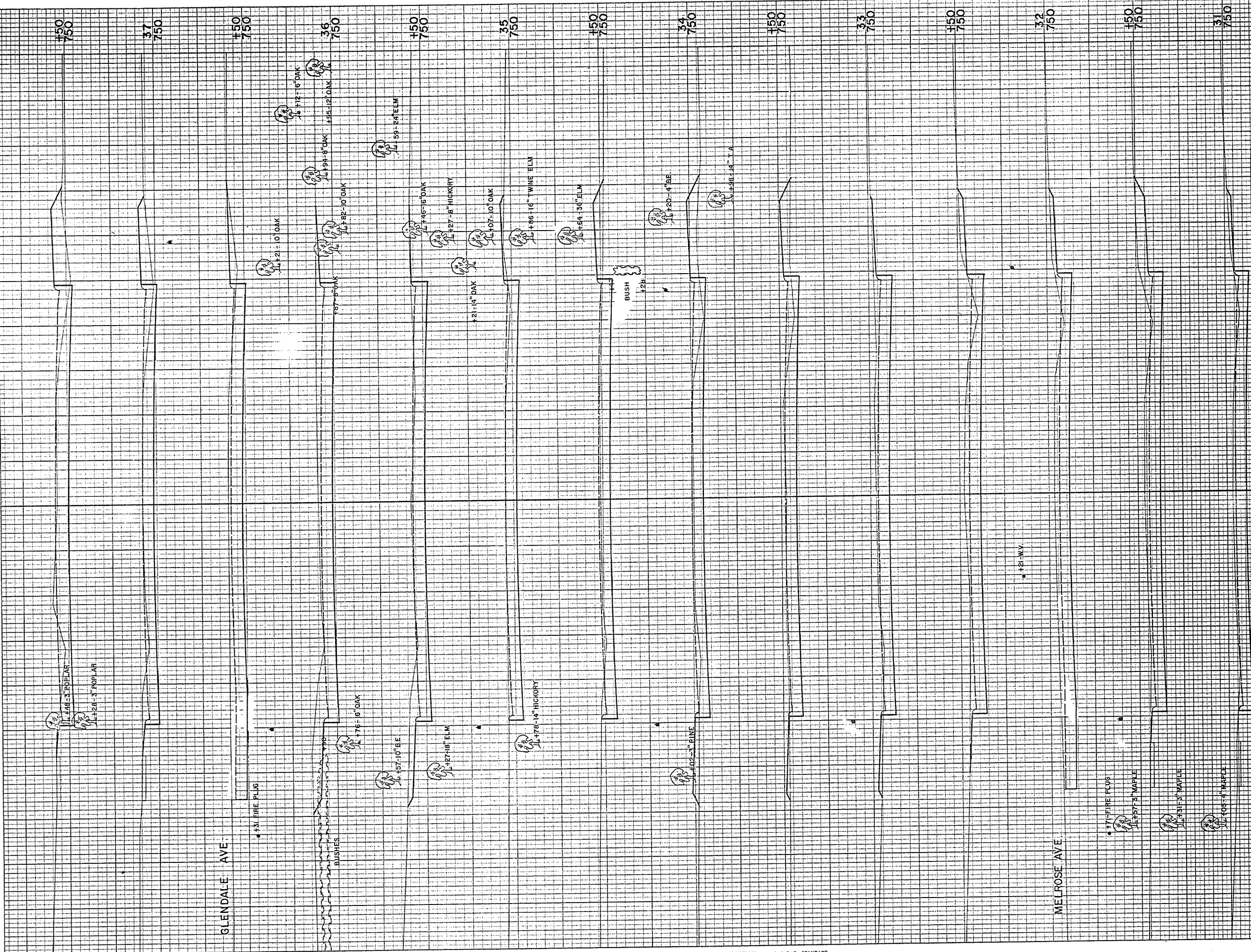
B.P.R. DISTRICT OFFICE WIS. 4
 PROJECT S 0324 (5)
 SHEET NUMBER 27
 TOTAL SHEETS 33



STATION	DISTANCE	YARDAGE	
		EXCAVATION	FILL
		UNCL.	
23	150	67	30
24		27	99
+244			
SECTION TOTAL = 3,865			871
24			
+244		27	99
150		59	191
25		59	189
150		68	157
26		100	60
150		120	39
27		91	46
150		85	28
28		85	26
150		85	18
29		104	6
150		115	9
30		115	11
150			
SHEET TOTAL		1,207	1,128

ORIGINAL SURVEY	DATE
NO. 390	10-61
DATE	12-61
BY	HANCOCK
BY	ANDERSON
BY	J. G. C.
BY	J. J. D.
BY	J. J.
DATE	1-62
BY	HANCOCK
BY	ANDERSON
BY	J. G. C.
BY	J. J. D.
BY	J. J.
DATE	1-62

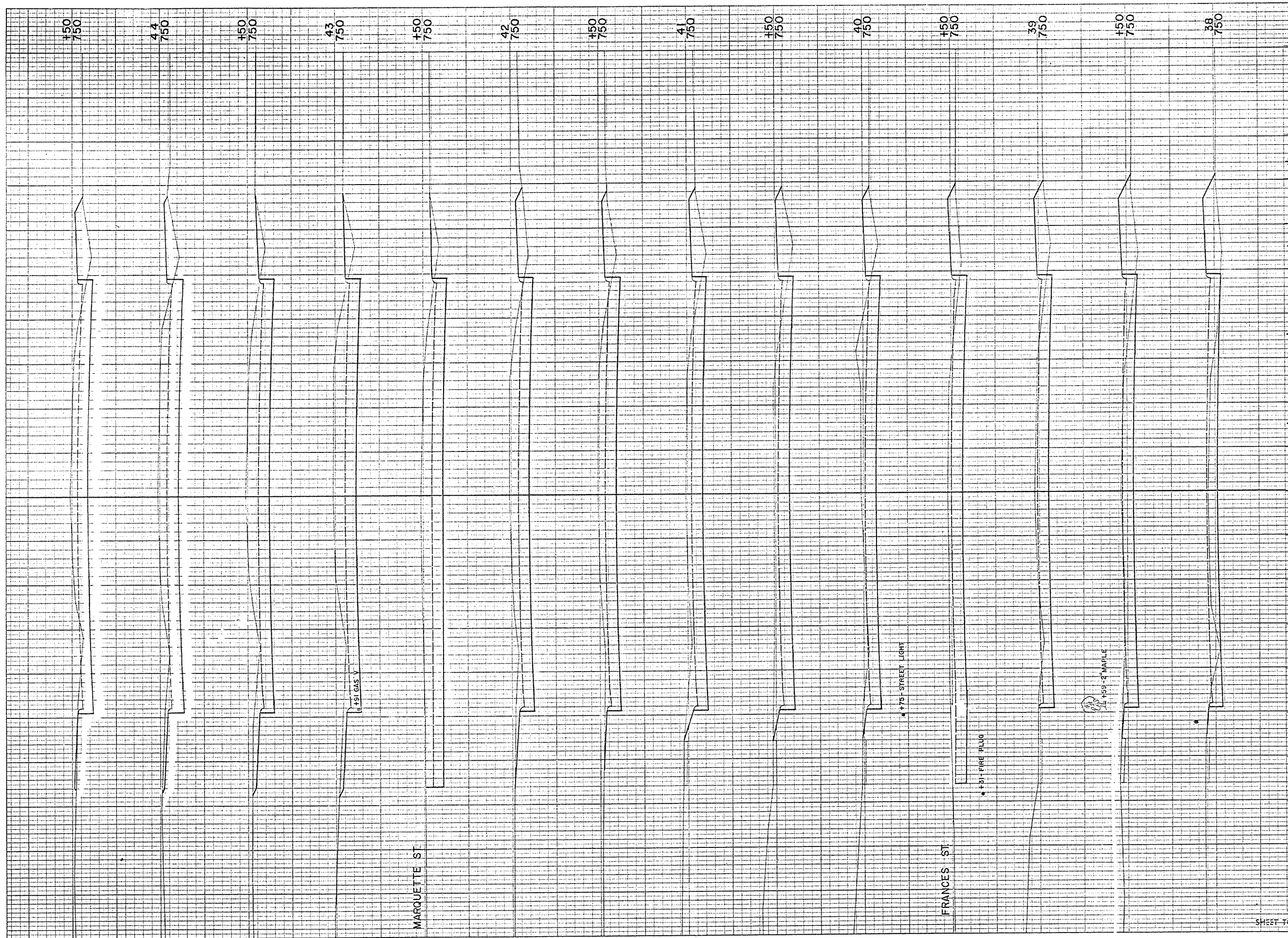
ORIGINAL SURVEY	DATE
NO. 390	10-61
DATE	12-61
BY	HANCOCK
BY	ANDERSON
BY	J. G. C.
BY	J. J. D.
BY	J. J.
DATE	1-62
BY	HANCOCK
BY	ANDERSON
BY	J. G. C.
BY	J. J. D.
BY	J. J.
DATE	1-62



STATION	YARDAGE	
	EXCAVATION	FILL
30	122	7
31	148	0
150	182	0
32	200	0
150	204	0
33	187	4
150	161	9
34	95	6
1352	SECTION ② TOTAL 2,412	
34	40	3
1352	128	7
35	139	2
150	163	0
36	157	6
150	117	15
37	100	28
150		
SHEET TOTAL		87

B.P.R. DISTRICT OFFICE	PROJECT	SHEET NUMBER	TOTAL SHEETS
WIS. 4	S 0324 (5)	28	33

S.P.M. DISTRICT OFFICE	PROJECT	SHEET NUMBER	TOTAL SHEETS
WIS. 4	S 0324 (5)	29	33



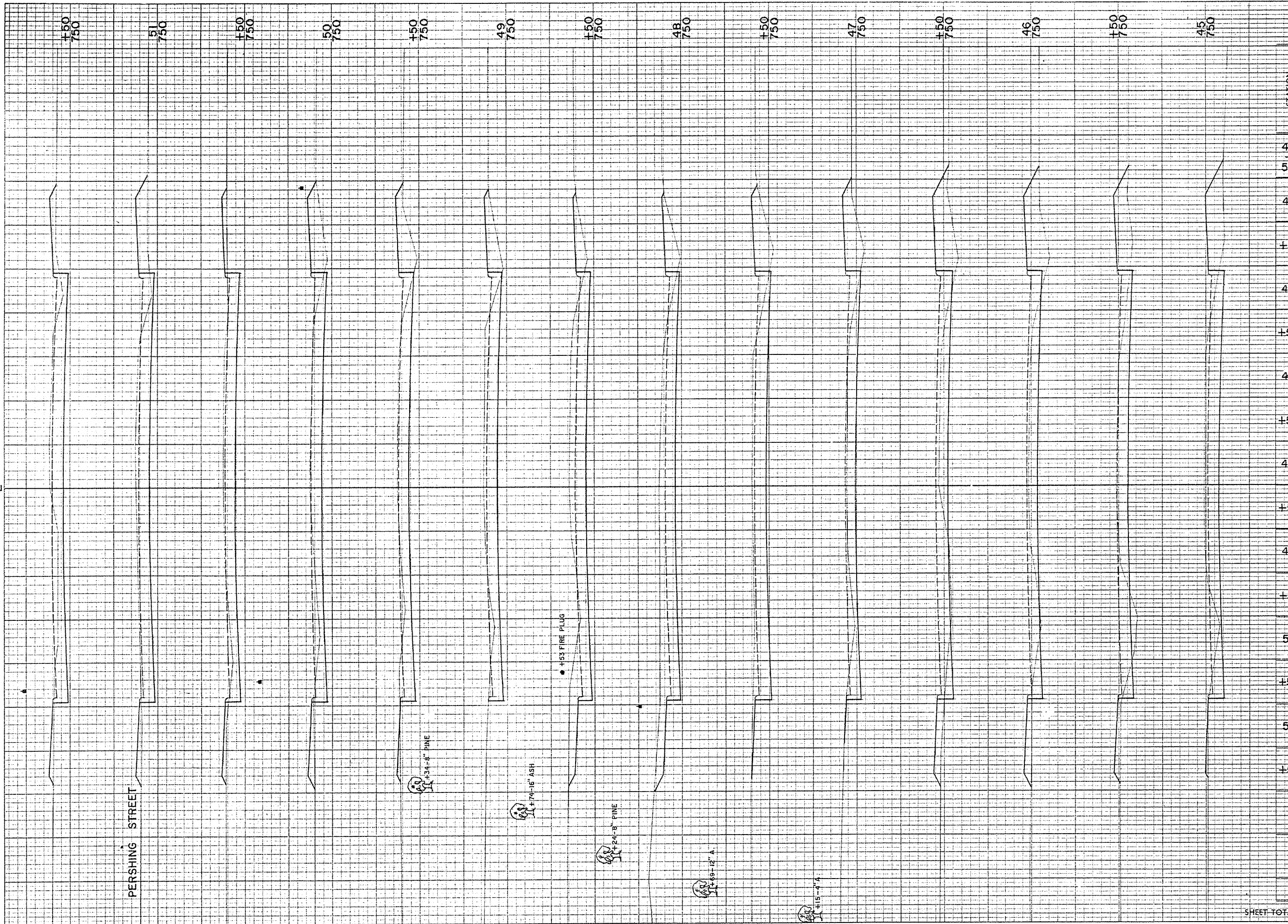
STATION	DISTANCE	YARDAGE	
		EXCAVATION	FILL
		UNCL.	
37+50		87	35
38		87	35
+50		109	30
39		128	22
+50		141	22
40		157	24
+50		161	24
41		157	22
+50		170	18
42		194	13
+50		206	9
43		200	11
+50		191	15
44		170	22
+50			

DATE: 10-6-02
 BY: J. H. F. C. O.
 ORIGINAL SURVEY: SURVEYED, PLOTTED, CHECKED, REVISIONS, AREAS CHECKED.
 NO. 390

DATE: 10-6-02
 BY: J. H. F. C. O.
 ORIGINAL SURVEY: SURVEYED, PLOTTED, CHECKED, REVISIONS, AREAS CHECKED.
 NO. 390

SHEET TOTAL 2,156 302

S.P.D. DISTRICT OFFICE	PROJECT	SHEET NUMBER	TOTAL SHEETS
WIS. 4	S 0324 (5)	30	33



STATION DISTANCE	YARDAGE	
	EXCAVATION	FILL
44+50	135	35
45	98	44
+50	68	48
46	52	54
+50	69	43
47	113	30
+50	157	24
48	174	18
+50	148	19
49	115	22
+50	106	33
50	103	26
+50	126	24
51	120	26
+50		
SHEET TOTAL	1595	446

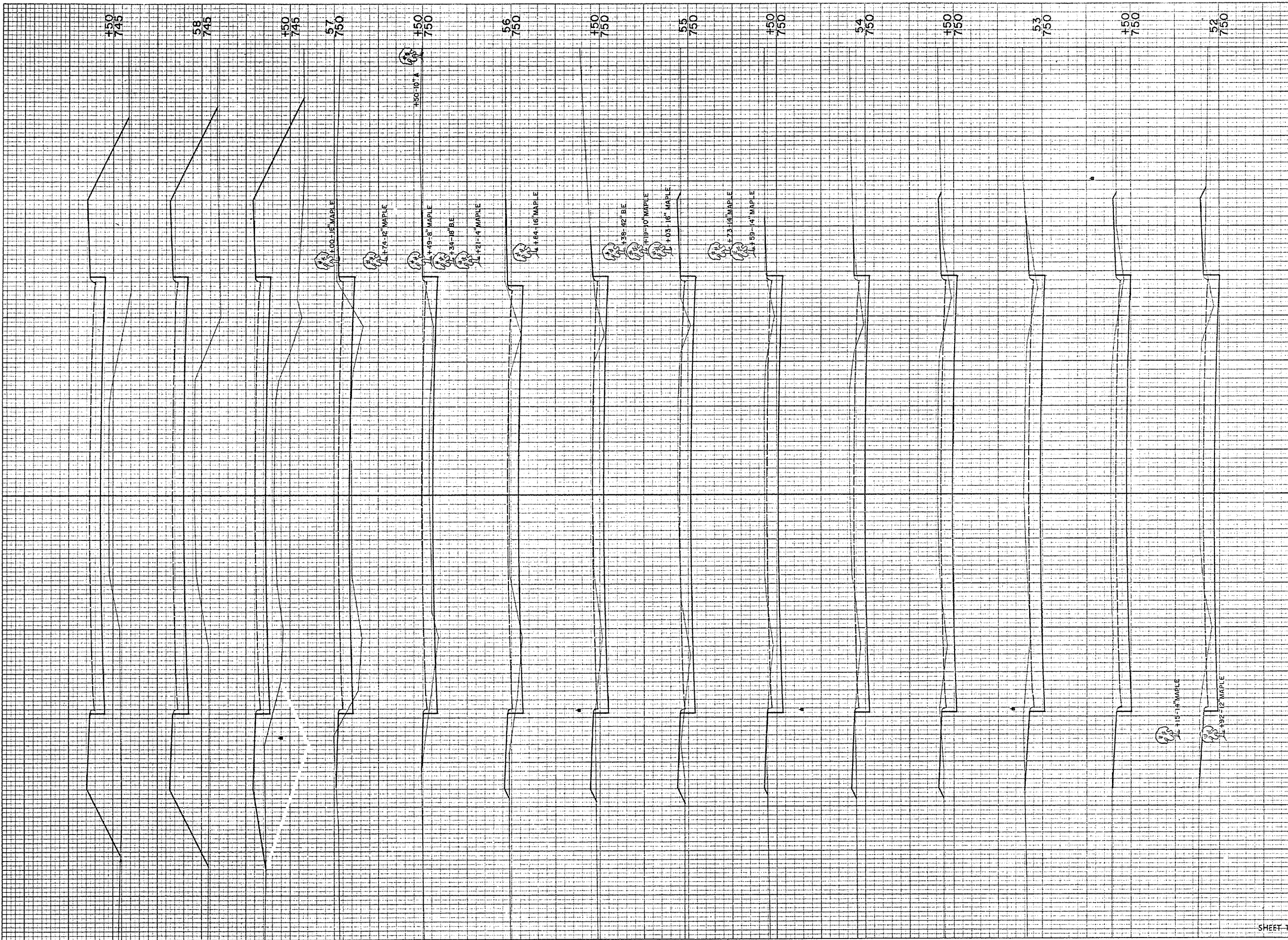
FINAL SURVEY
 GRANITE
 NORTH LONDON
 ARMS
 AREA: CHICAGO
 NO.

ORIGINAL SURVEY
 GRANITE
 NORTH LONDON
 ARMS
 AREA: CHICAGO
 NO. 390

BY
 R. F. H.
 J. D. O.
 J. D. O.

FINAL SURVEY NO. 390
 SURVEYED BY J. E. D.
 PLOTTED BY J. E. D.
 DATE 12-61

ORIGINAL SURVEY NO. 390
 SURVEYED BY J. E. D.
 PLOTTED BY J. E. D.
 DATE 12-61



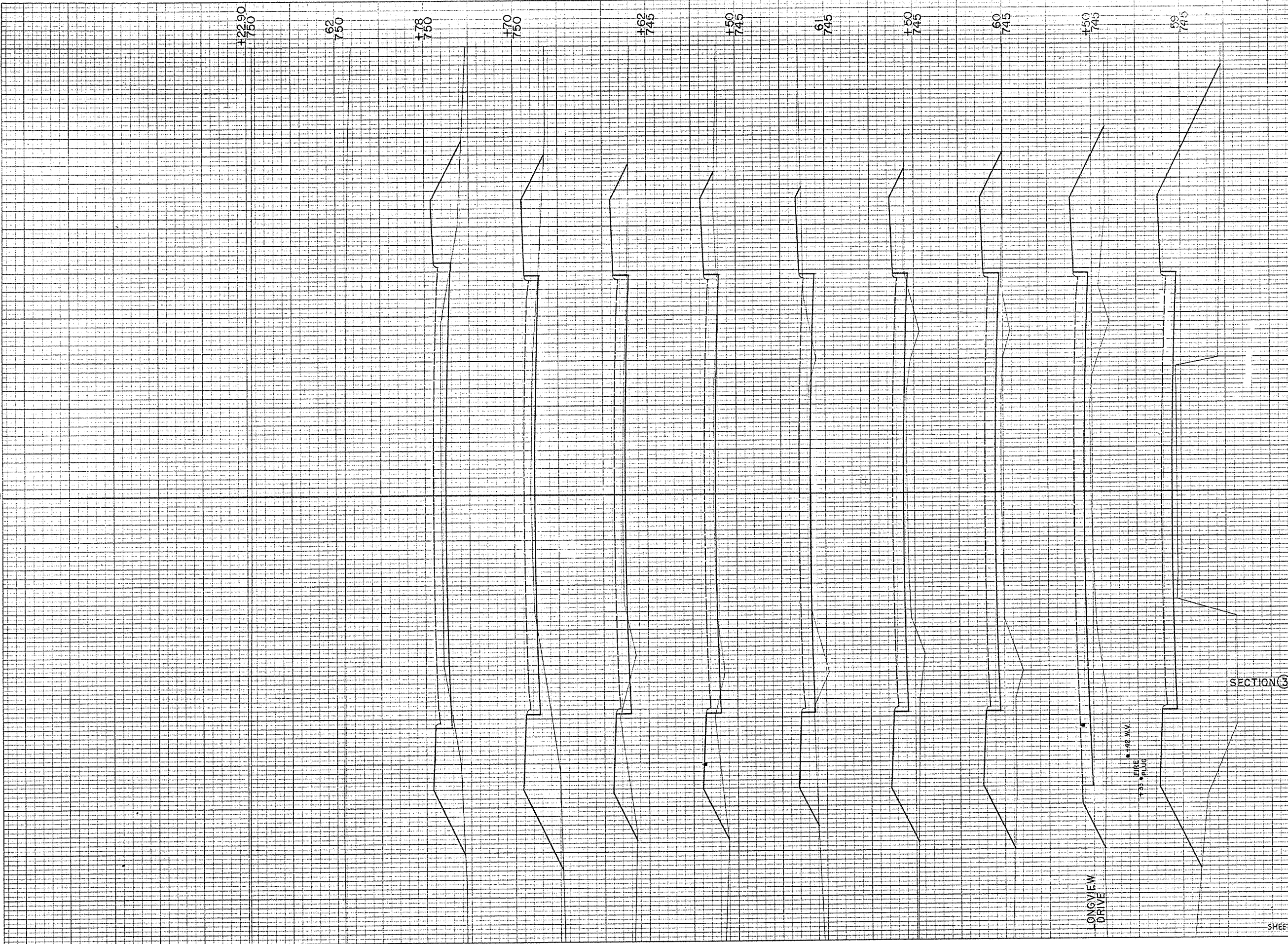
STATION	DISTANCE	YARDAGE	
		EXCAVATION	FILL
51	150		
52	119		15
53	148		9
54	167		7
55	154		9
56	139		11
57	137		6
58	128		7
59	109		11
60	91		11
61	59		9
62	24		26
63	0		263
64	0		444
65	0		381
SHEET TOTAL	1,281		1,209

R.P.R. DISTRICT OFFICE WIS. 4
 PROJECT S 0324 (5)
 SHEET NUMBER 31
 TOTAL SHEETS 33

S. P. DISTRICT OFFICE	PROJECT	SHEET NUMBER	TOTAL SHEETS
WIS. 4	S 0324 (5)	32	33

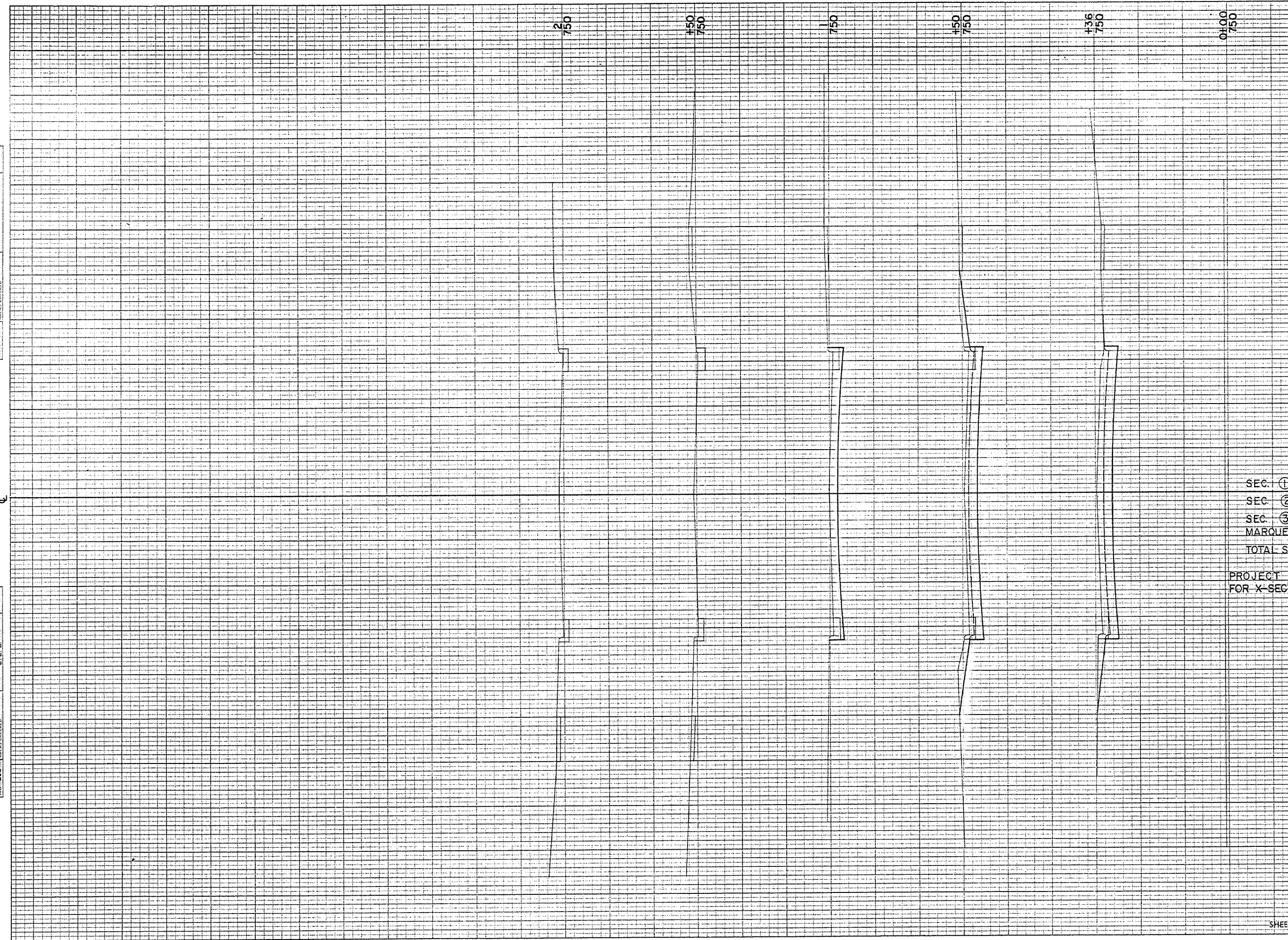
FINAL SURVEY
 NOTE BOOK NO. 390
 AREAS CHECKED

ORIGINAL SURVEY
 NOTE BOOK NO. 390
 AREAS CHECKED



STATION	DISTANCE	YARDAGE	
		EXCAVATION	FILL
58	150	0	435
59	0	0	432
+50	0	0	254
60	0	0	193
+50	6	6	117
61	11	11	70
+50	4	4	17
+62	6	6	13
+70	7	7	22
+78	11	11	26
62	0	0	0
+12			
SECTION ③ TOTAL		5,921	3,675
SHEET TOTAL		45	1,657

S.P.R. DISTRICT OFFICE	PROJECT	SHEET NUMBER	TOTAL SHEETS
WIS. 4	S 0324 (5)	33	33



STATION	DISTANCE	YARDAGE	
		EXCAVATION	FILL
0			
+33			0
+36			0
	30		0
+50			0
	75		0
+100			

SUMMARY		
SEC. ①	3,865	871
SEC. ②	2,412	925
SEC. ③ BALLARD	5,921	3,675
MARQUETTE ST	113	0
TOTAL SEC. ③	6,034	3,675
PROJECT TOTAL FOR X-SECS.	12,311	5,471

FINAL SURVEY NOTE BOOK NO. 390

DATE 1-6-62
BY D. H. W. R. N. L. L. B.